

# GATEWAY CENTER WEST

***GATEWAY CENTER  
WEST  
MASTER PLAN  
AND  
DESIGN GUIDELINES***

**PREPARED FOR  
SOUTHEAST ECONOMIC DEVELOPMENT CORPORATION**

**BY  
HCHP/RNP/WRT  
A COLLABORATION**

**September 1992**

*GATEWAY CENTER  
WEST  
MASTER PLAN AND DESIGN GUIDELINES*

TABLE OF CONTENTS	PAGE
I. BACKGROUND	2
II. PLANNING OBJECTIVES	3
III. EXISTING CONDITIONS	4
IV. PROJECT DESCRIPTION	6
V. ARCHITECTURAL STANDARDS	11
A. Architectural Theme	11
B. Industrial Zoned Developments	13
C. Commercially Zoned Developments	15
D. Building Heights	15
E. Special Study Area: Commercial Center	15
F. Special Study Area: SDG&E Site	15
G. Special Study Area: Archie Moore Site	18
H. Renovation of Existing Buildings	18
I. Light Standards	19
J. Sign Guidelines	20
K. Security Fencing and Walls	23
L. Outdoor Storage	23
M. Color and Materials	24
VI. LANDSCAPE STANDARDS	26
A. Landscape Concept	26
B. East-West Corridors	26
C. North-South Corridors	32
D. Intersection Treatment	36
E. Sunrise Street	38
F. Freeway Boundary	40
G. Landscape Treatment of Individual Property	41
H. Proposed Plant Materials	42
I. Plant Material for Individual Lot Development	44
VII. PERMITTED USES	45
A. Permitted Use by Zone	46
B. Industrial Zone Development Standards	51

VIII.	SUBMITTAL AND REVIEW PROCESS	55
	A. Agency Projects	55
	B. Other Projects	56
IX.	INFRASTRUCTURE	57
	A. Existing Conditions	57
	B. Proposed Project	58

## LIST OF EXHIBITS

EXHIBIT NUMBER	PAGE	
1.	Gateway Center Location Map	1
2.	Airphoto of Existing Site	5
3.	Preferred Concept Plan	9
4.	Axonometric View	10
5.	Medium Lots	12
6.	Commercial Development Graphic	14
7.	SDG&E Building	16
8.	Special Study Area	17
9.	Street Wall Definition	22
10.	Color/Material Matrix	24
11.	E and F Street	27
12.	Pickwick Avenue	29
13.	33rd Street and Pickwick Avenue	33
14.	F Street and 32nd Street	35
15.	Sunrise Street Pedestrian Corridor	37
16.	I-15 Boundary	39
17.	Zoning Map	53
18.	Land Use Map	54
19.	Infrastructure and Lot Improvements	60
20.	Utilities Map	61

The Gateway Center West Design Guidelines have been prepared for the Southeast Economic Development Corporation by:

HCH PARTNERS  
4877 Viewridge Avenue  
San Diego, CA 92123

Phone: (619) 278-5750

Contact: Anthony Ambrose

ROESLING NAKAMURA ARCHITECTS, INC.  
363 Fifth Avenue  
San Diego, CA 92101

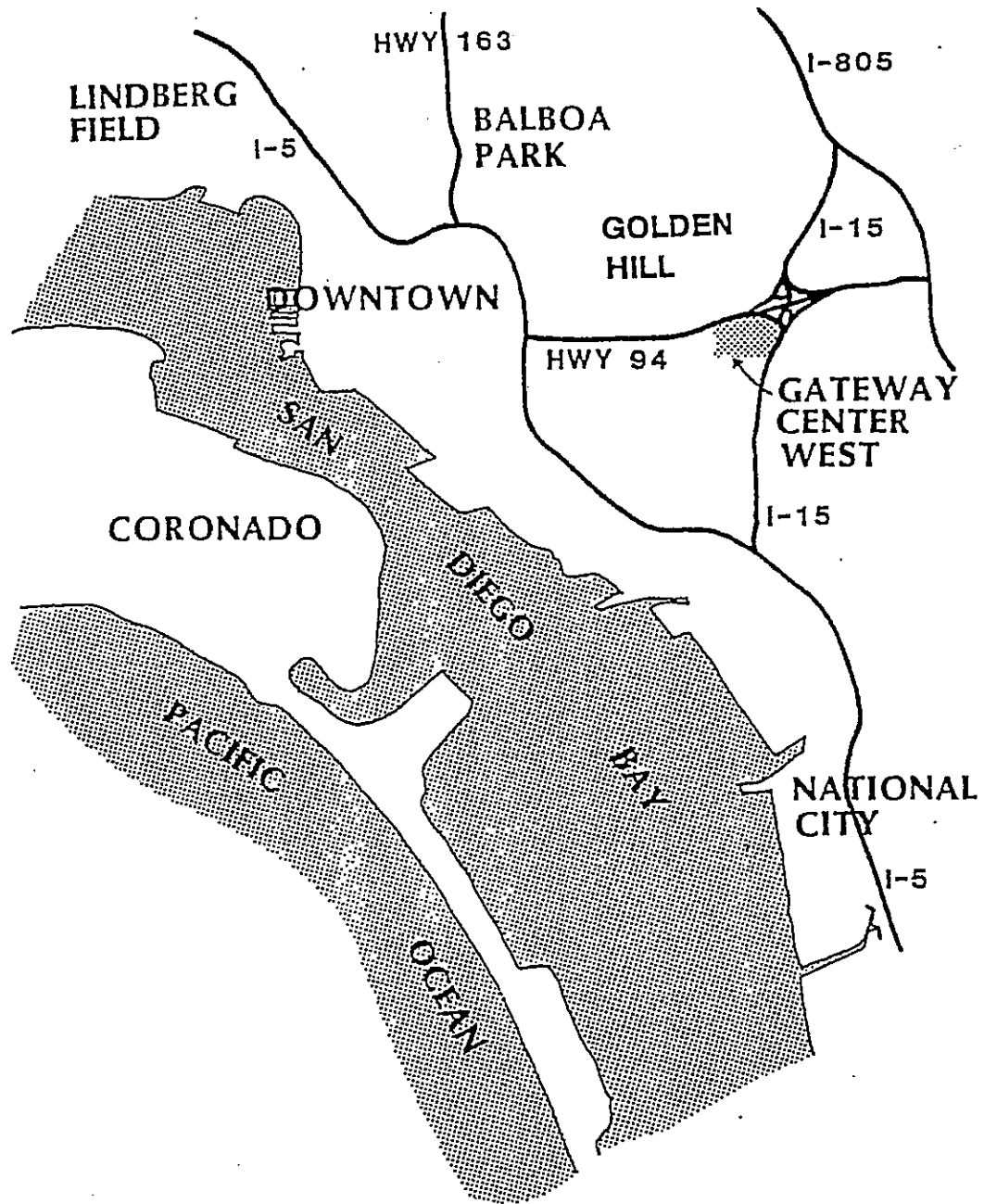
Phone: (619) 233-1023

Contact: Ralph Roesling

WALLACE ROBERTS AND TODD  
1133 Columbia Street, Suite 205  
San Diego, CA

Phone: (619) 696-9303

Contact: Ignacio Bunster



*Gateway Center West  
Location Map*



## I. BACKGROUND/INTRODUCTION

Strategically located at the southwest corner of Interstate 15 and the Martin Luther King Jr. Freeway, (Highway 94); the approximately 61 acre Gateway Center West project site is truly at the crossroads and a major entry point to the Fourth District Community of San Diego. Since it is only two miles from downtown along the Martin Luther King Jr. Freeway, the site enjoys both the amenities and the conveniences of downtown, yet it is far enough away to escape its traffic congestion. Further, industrial land values within the project area are considered very reasonable.

Gateway Center West is the western portion of a much larger redevelopment area known as Gateway Center. Until 1985 Gateway Center West was a part of the Dells Redevelopment Area. Gateway Center East is nearly built out. The Gateway Center West project area has an existing mixed land use consisting of residential, industrial and commercial uses. The purpose of these master plan and design guidelines is to guide existing business owners within the project area, future businesses, developers, builders and SEDC staff in creating a redevelopment project which is consistent with the goals and objectives of the agency and the community plan.

The Design Guidelines represent an effort to unify Gateway Center West's very unique land use configuration. Unlike the East, which was essentially vacant when redeveloped, the West has numerous existing industrial businesses and residences. The residential uses and some of the business uses are legal non-conforming uses. These guidelines provide suggested design elements that when implemented will unify the streetscapes with the existing and new construction.

These guidelines have been prepared to provide a road map for the completion of the Gateway Center West redevelopment project area. The guidelines have been prepared with the input from SEDC staff, the effected property owners, the Southeast Development Committee and the consultant team. The consultant team is comprised of HCH Partners (HCHP), lead planner and civil engineer; Roesling Nakamara Architects, Inc. (RNP), architect; and Wallace Roberts & Todd (WRT) planner and landscape architect. This planning effort began in late 1991.

These guidelines are written as explicitly as possible, recognizing the fact that they will be enforced with a good deal of discretion. The document is organized into nine different sections. These sections include: Background/Introduction; Planning Objectives; Existing Conditions; Project Description; Architectural Standards; Landscape Standards; Permitted Uses; Submittal and Review Process and Infrastructure. The sections that provide the unifying design elements are architectural and landscape standards. Combined, the two sections provide guidance for the ultimate streetscape within Gateway Center West. The Architectural Standards section establishes the relationship of buildings to the site. Within this section are several subsections detailing site planning and architecture. Further, the architectural section establishes the architectural theme, massing and scale requirements and special study areas within Gateway Center West. The landscape section establishes the landscaping theme for Caltrans and local street right-of-way and individual site development landscaping. The infrastructure section summarizes the existing infrastructure conditions and recommendations for redevelopment.

## II. PLANNING OBJECTIVES

Subsequent to a review of previous planning studies and discussions with SEDC staff and local property owners, several planning objectives were developed. The objectives for the Gateway Center West Design Guidelines are as follows:

- To be consistent with the objectives of the Community Plan
- To create high quality industrial/Business Park that will harmonize with Gateway Center East.
- To transition non-conforming uses currently within the project area.
- To define Gateway Center's natural boundary and transition areas.
- To promote an economically viable business park.
- To provide a safe pedestrian environment.
- To provide guidance to property owners of existing buildings and/or vacant lots.
- To create harmony and unity between various lots.
- To provide a variety of business opportunities.
- To identify the improvements necessary to modernize the existing infrastructure.
- To inspire existing and future property owners.

The above objectives are distillations of the input the consultant team received from numerous meetings with SEDC staff, the effected property owners and one workshop/presentation with the Community Planning Group.

### III. EXISTING CONDITIONS

The project encompasses approximately 58 acres with over 90 legal parcels. The project area has a long history which has contributed to the variety of parcel sizes and mixed land uses. In the late 1880's, Daniel Choate first subdivided the land and created a grid of 300 x 600 foot rectangular shaped blocks. The individual lots were 25 feet wide by 140 feet deep served by a back alley. The project area was originally developed as a residential area and retained that characteristic until the late 1940's. During the mid-1950's a transition from residential to industrial and commercial uses was apparent. In fact, some of the businesses that currently operate within the project area are located in houses converted to industrial and commercial uses.

Existing development along F Street between 32nd Street and 33rd Street is a mix of residential, commercial and industrial. Several single-family detached homes are concentrated within the vicinity of the intersection of 33rd and E Street. Development along E Street is composed of a mix of residential, commercial and industrial uses. E Street also contains numerous visible contractor outdoor-storage yards. SDG&E is the most visually predominant property with its acres of outdoor storage. The new construction primarily located around Market Street and 33rd Street intersection is modern and contemporary. However there is no unifying architectural theme or streetscape for the entire Gateway Center West project area.

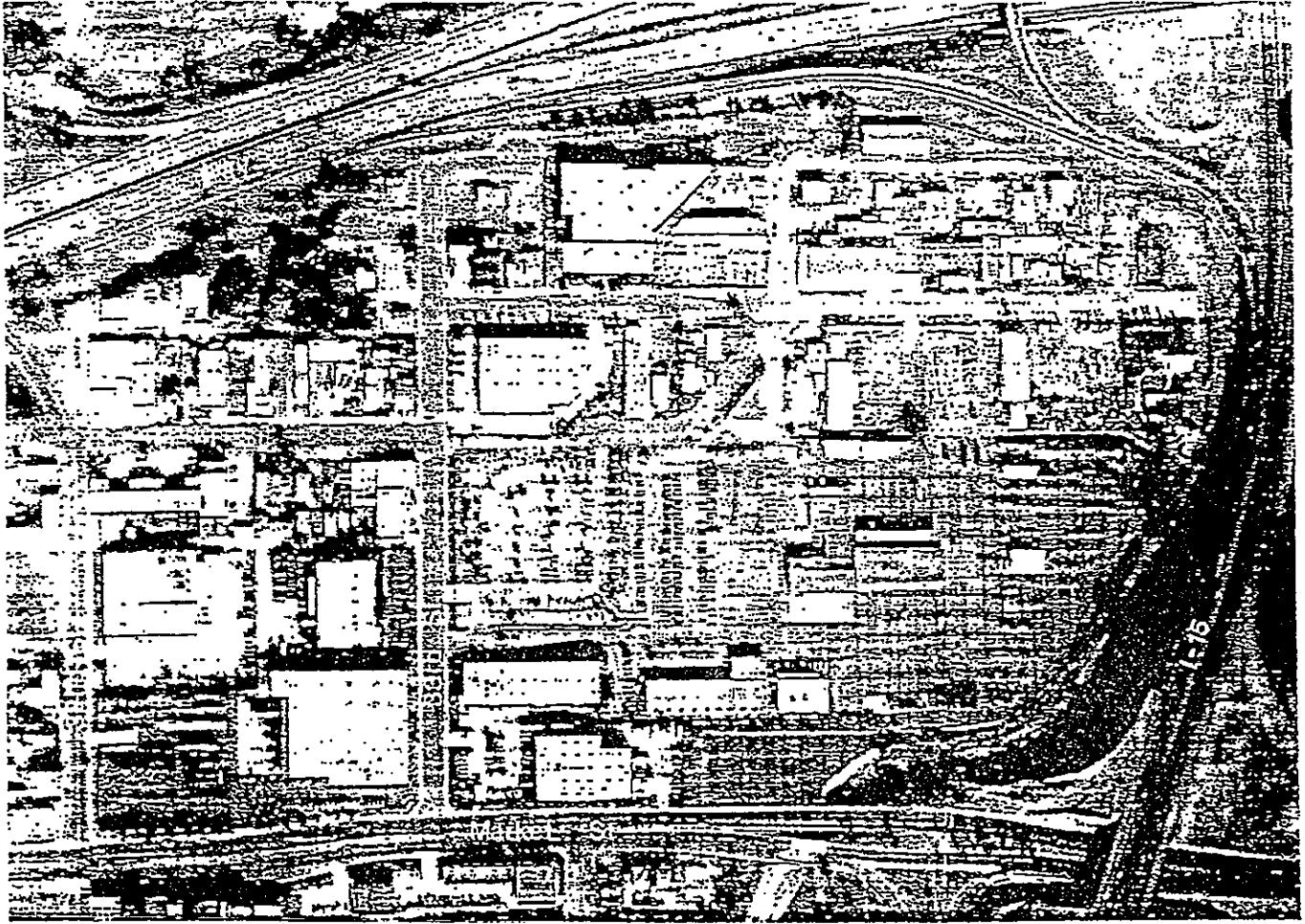
The project area currently consists of a variety of residential, old industrial, new industrial (since 1985), vacant land, SDG&E (Center City operating station), open space and streets. The following chart breaks down the aforementioned uses:

<u>Land Use</u>	<u>Acerage*</u>	<u>% of Total</u>
Residential	4.25	7.3%
Old Industrial**	10.00	17.0%
Redeveloped Industrial	14.50	24.7%
SDG&E	16.00	27.3%
Open Space	1.35	2.3%
Vacant	1.50	2.6%
Street Right-of-Way	11.00	18.8%
<u>TOTAL</u>	<u>58.60</u>	<u>100%</u>

\* Approximate sizes.

\*\* Those buildings/businesses created prior to 1985.

Presently, the single largest parcel is owned by San Diego Gas & Electric Company. This parcel is approximately 16 acres and is known as the Center City Operating Station. Approximately 14.5 acres of the project area have been redeveloped within the past decade.



*Airphoto of Existing Site*

#### IV. PROJECT DESCRIPTION

Gateway Center West is located in the south-western corner of the interchange of the Martin Luther King Jr. Freeway (Highway 94) and Interstate 15. The project area is bounded by 32nd Street on the west, Market Street on the south, Interstate 15 on the east and Martin Luther King Jr. Freeway on the north. The primary access to the site is via Market Street, a major east-west urban thoroughfare serving the community and extending directly into downtown. Access to the site is also provided by 32nd Street, a north-south urban collector which becomes Broadway north of the Martin Luther King Jr. Freeway. Broadway also provides a direct link to downtown. Freeway access is provided from Market Street (which has a full intersection with I-15) and 32nd Street (which provides westbound access to Highway 94).

The majority of the project area is nearly developed. There are approximately 90 legal parcels within the project area. As has been previously stated, the largest property owner is San Diego Gas & Electric with 16 acres. There are approximately 23 residential parcels with an unknown number of actual residents. The balance of the parcels are being used for industrial or commercial uses. Six properties totaling 14.5 acres have been redeveloped into new industrial uses since 1985. These redeveloped properties have created nearly 200,000 square feet of new industrial product.

##### Concept Plan

The current preferred concept plan for Gateway Center West is the result of meetings with the SEDC staff, existing property owners and the Southeast Development Committee. The preferred concept plan represents the ultimate street pattern and lot configuration. The plan envisions the entire site will become an industrial use with no non-conforming uses. Two parcels, however, are planned for special uses. Parcel Number 1 (at the terminus of E Street) is planned as a gymnasium and/or athletic club as its primary use. The site is referred to as the Archie Moore Special Study Area. The alternative use is industrial. Parcel Number 8 (the triangular property and building located east of Sunrise Street) is planned as a commercial support site for the project and is referred to as the Archie Moore Special Study Area.

The project design theme is to create a streetscape that is pedestrian friendly. This theme is developed through the architectural standards and the landscaping standards. The architectural standards provide for a "street wall" concept for both new and existing developments. The purpose of the "street wall" concept is to unify Gateway Center West's existing and new developments. The "street wall" concept defines the public space of the streetscape and provides identity to private development. This concept involves orienting the buildings toward the street to increase visibility and business access. The landscape standards enhance the "street wall" concept by creating a unified landscape image.

The "street wall" concept involves pulling new construction closer to the street similar to the existing traditional development along F Street. This concept for Gateway Center West will permit a finer integration of traditional development with newer development. New building construction will maintain a minimum 10 foot setback from the right-of-way. Architectural walls or wrought iron fencing or combinations of the two are permitted at the property or right-of-way line. These walls/fences are not to enclose the building street facade. The purpose of the wall/fences is to screen and secure outdoor storage and parking areas. Building walls and wall/fences will provide variable vehicle features that will work in tandem with the landscaping to provide a pleasant and secure pedestrian environment.

The project area will consist of the following land uses when completed:

<u>Land Use</u>	<u>Acreage</u>	<u>% of Total</u>
New Industrial*	10.40	17.7
Potential Industrial Rehabilitation	5.25	9.0
Existing Upgraded Industrial*	14.50	24.7
Special Study Areas		
Archie Moore	1.40	2.3
Commercial Center	.30	.5
SDG&E	16.00	27.3
Residential	.7	1.2
Open Space	1.35	2.3
Street Right of Way	8.70	15.0
<b>TOTAL</b>	<b>58.60</b>	<b>100.0%</b>

\* Built since 1985 and meets current development standards

Several alternative project designs were considered by the consultant team and SEDC staff. The alternatives generally involved the following: Looping Pickwick Street back to E Street; extending 33rd Street, as a cul-de-sac, north to the existing Pickwick Street; and combining Lots 8 and 9. The alternatives were considered and eventually rejected because they resulted in less developable acreage and or higher potential site improvement costs.

### Circulation Element

The proposed project will require minor modifications to the existing vehicular circulation system. These modifications involve: A cul de sac at the eastern terminus of E Street and Pickwick Avenue; a knuckle at 33rd and E Street; the closure or vacation of 35th Street; the closure of Sunrise Street; the closure of 34th Street south of E Street; and improvement of the alley north and parallel to F Street between 32nd and 33rd Street. Sunrise Street will be converted to a pedestrian plaza. This plaza will permit emergency vehicular access when necessary.

### Lot Configuration

The aforementioned modifications to the vehicular circulation system sets the framework for the revised lot configurations. Lots 1 through 5 (see preferred concept plan, Exhibit 3 on page 9 will provide 4 industrial lots that are 1/2 acre or less and 1 lot (Lot Number 1) approximately 1 1/2 acre in size. Those lots less than 1/2 acre have the ability of being consolidated with adjacent similar sized parcels if a larger parcel is warranted. Lots 6 and 7 are existing parcels that will benefit by consolidation of adjacent similar ownership into 1 acre plus sized lots. Lot 8 will be expanded in size by the closure of Sunrise and 34th Streets. Lots 9 through 15 involve numerous non-conforming uses and redevelopment of these parcels into industrial uses is envisioned. Lots 10 and 11 will provide a 1 acre and 1.5 acre lot, respectively, and can be consolidated.

### Zoning

The project area East of 32nd Street is zoned for industrial uses (See Zoning Map, Exhibit 17, page 53). The majority of the area is zoned I-2. The SDG&E property is zoned I-1.

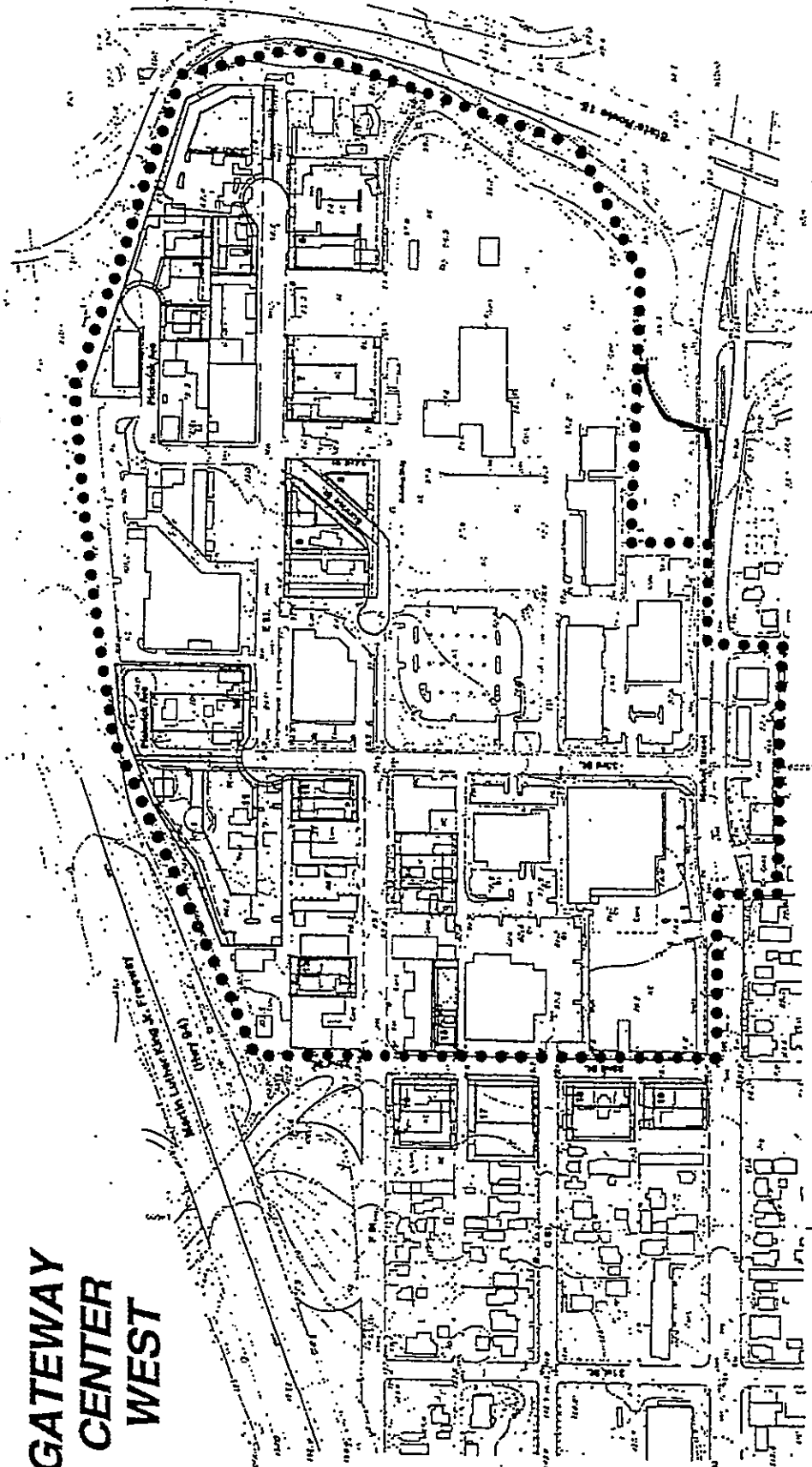
The I-1 zone provides for a wide range of manufacturing, light industrial and some heavy commercial uses. The I-2 zone provides criteria for larger industrial complexes and permits office development that is related to the industrial use. The project is consistent with the existing zoning in this area.

The proposed expanded project area east of 32nd Street is zoned MF-3000, CSF-2 and CSF-3. The CSF zone provides for commercial strip development that is primarily automobile-oriented. Lots 16 and 17 are zoned CSF-3. Lot 19 is zoned CSF-2. Lot 18 is zoned MF-3000, a multi-family residential zone. Inclusion of the expansion area will require rezoning to the I-1 or I-2 zones. An alternative option to rezoning the expansion area to industrial would be to rezone Lot 18 to CSF-2. This would be consistent with adjacent CSF zones.

### Future Expansion

Expansion of Gateway Center West into the adjacent Dells Redevelopment Area was considered. The result of the preliminary investigation into expansion was to recommend a modest westerly expansion of Gateway Center West. This westward expansion involves Lots, 16, 17, 18 and 19. Lot 17 is currently vacant and Lot 19 has an existing commercial application. Only Lots 16 and 18 are considered non-conforming uses. The expansion will secure all of 32nd Street within the project area. Inclusion of the west side of 32nd street permits Gateway Center West to have control of the street scene and adjacent uses. Existing residential areas within the Dells will be buffered by required landscape setbacks. Further, the agency will enforce these design guidelines and land uses to insure compatibility.

# GATEWAY CENTER WEST



Project Boundary



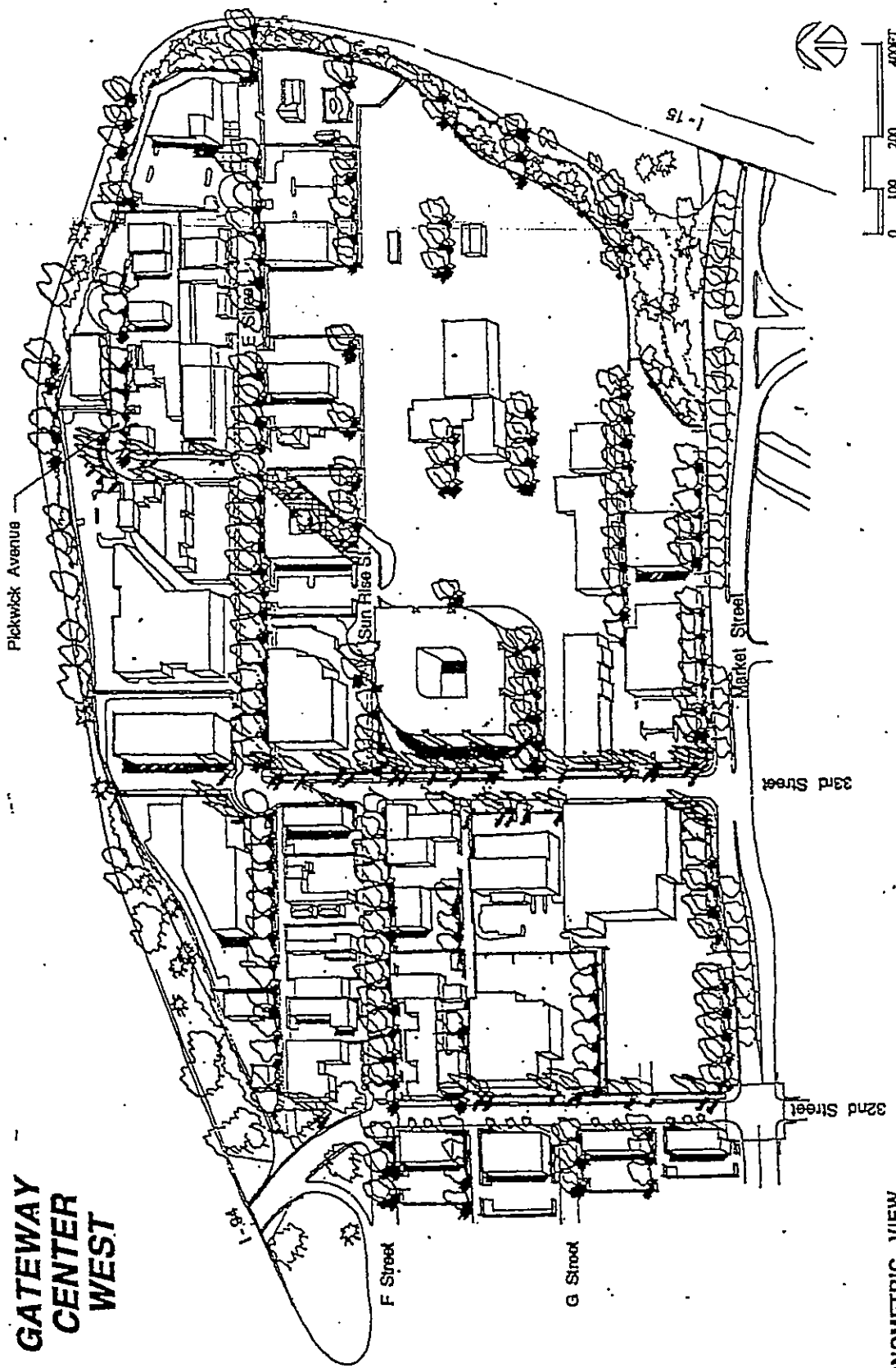
Preferred Concept Plan



4177 Viewridge Avenue  
San Diego, CA 92131 (619) 215-6100  
FAX: (619) 215-6104  
March 1997

Architectural

# GATEWAY CENTER WEST



AXONOMETRIC VIEW

## V. ARCHITECTURAL STANDARDS

### A. Architectural Theme

The architectural concept contained in the plan is based on the desire to create pedestrian space at the streetscape edge. Another prime objective is to encourage the use of design elements that allow a transition of scale from smaller to larger developments.

#### 1. Industrial Developments

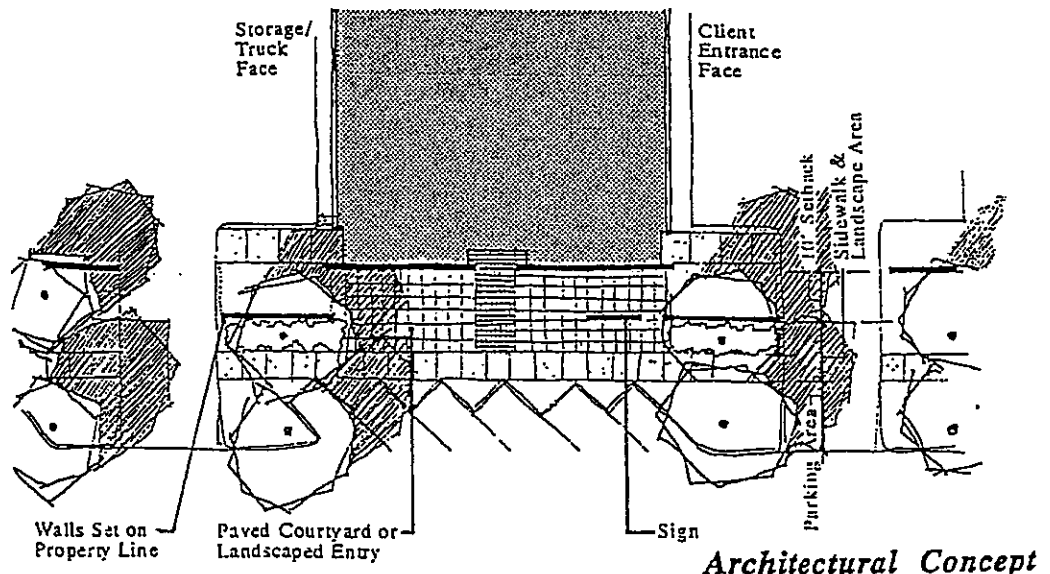
The architectural concept as shown in sketch below delineates a rhythm of wall planes near and at the edge of the public right of way. These walls and fences enhance and define the public streetscape zone. The buildings front facades, adjacent walls and fences are located either at the property line or at a 10 foot setback in order to create a "street wall" expression. This "street wall" helps to define the public space of the streetscape and gives identity to the private development.

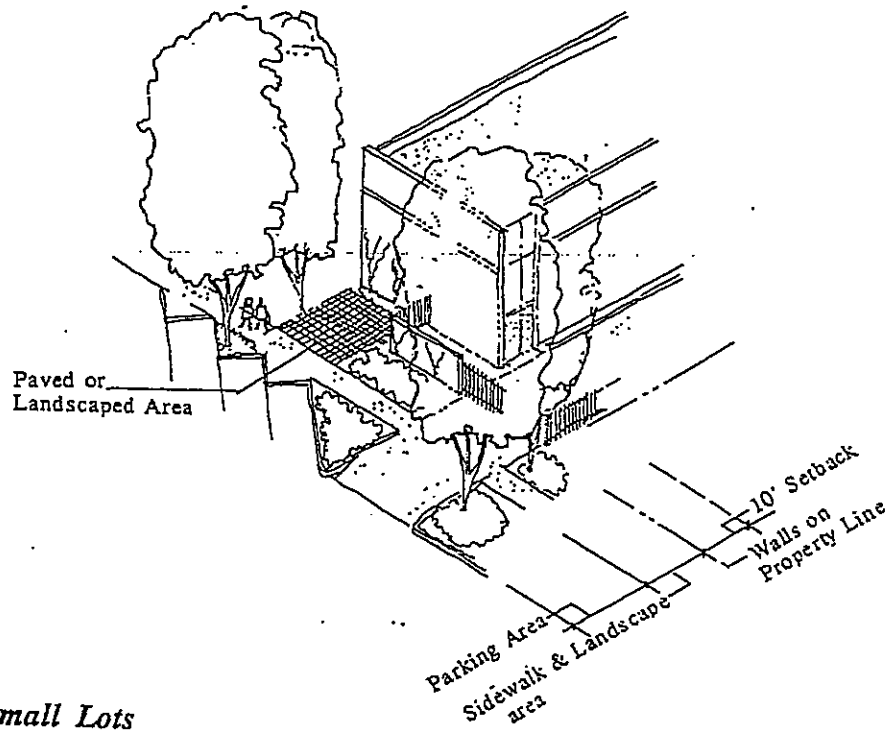
The interior zone of each private development lot is less regulated allowing for flexibility needed in commercial and industrial development. Guidelines are set for the enhancement of the client / visitor side of buildings as well as the truck / storage side of buildings.

An example of the successful definition of public and private space is in the south Gaslamp Quarter of downtown San Diego. This part of the Gaslamp Quarter is mostly warehouse uses. These structures are generally at the property line next to the sidewalk and define a comfortable public pedestrian zone.

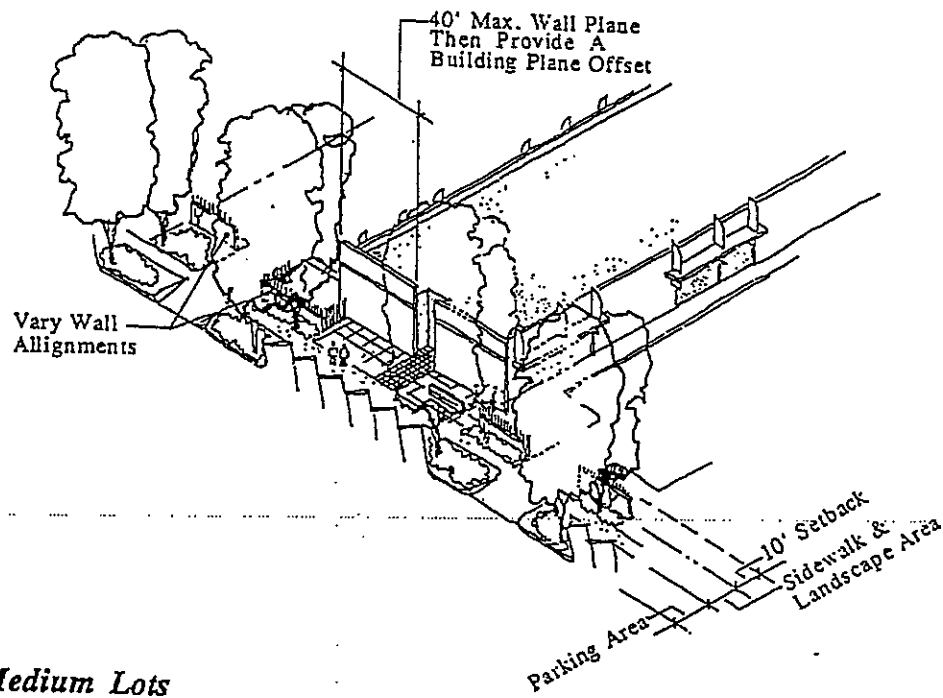
#### 2. Commercial Developments

The commercial architectural theme is intended to allow buildings to engage the pedestrian by moving the storefront to the property line. Storefront openings and windows are encouraged along the front and around corners to create transparency in and out of lease space. Architectural projections such as awnings, blade signs, and canopies at the front facade are encouraged to add pedestrian scale and interest to the streetscape.





*Small Lots*



*Medium Lots*

## B. Industrial Zoned Developments

### 1. Small Lots

The design guidelines for smaller lots (approximately 50'-100' frontage x 150' deep), are intended to allow for development flexibility. One of the planning objectives for small lots is to allow for efficient lot utilization by permitting a 10' front yard setback, and minimal or no setbacks at the sides and rear yards..

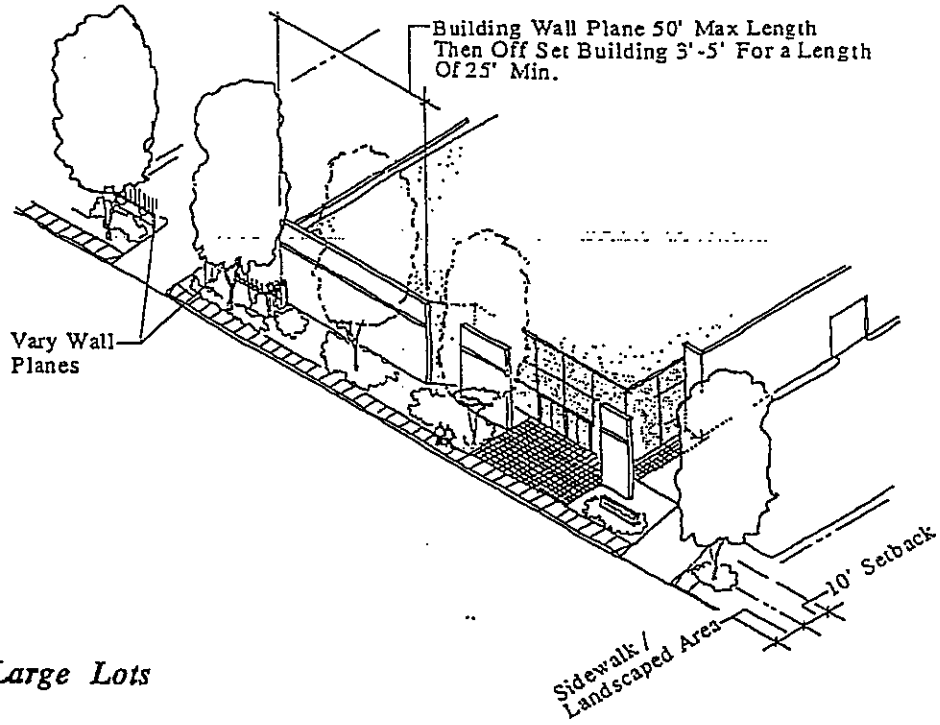
Where small existing structures occur that conform to all Planned District Ordinance (PDO) land use and planning criteria, they shall be renovated to these design standards.

- The building facades are encouraged to be simple wall planes with no offsets along the front setback. This serves the purpose of defining the public streetscape zone and creates an identity wall for signage.
- Entries to buildings should be articulated by enhanced paved areas and architectural features such as canopies, or storefront glazing.
- Fencing also delineates another wall plane at the property line.
- A transition of scale from the small lots to other adjacent development is provided through the use varied wall planes fencing. (See Small Lot Sketch on opposite page)
- Continuity between the industrial and commercial properties will be maintained through the use of an accent stripe and architectural projections at matching heights. (See Renovation Guidelines Sketch on page 18)

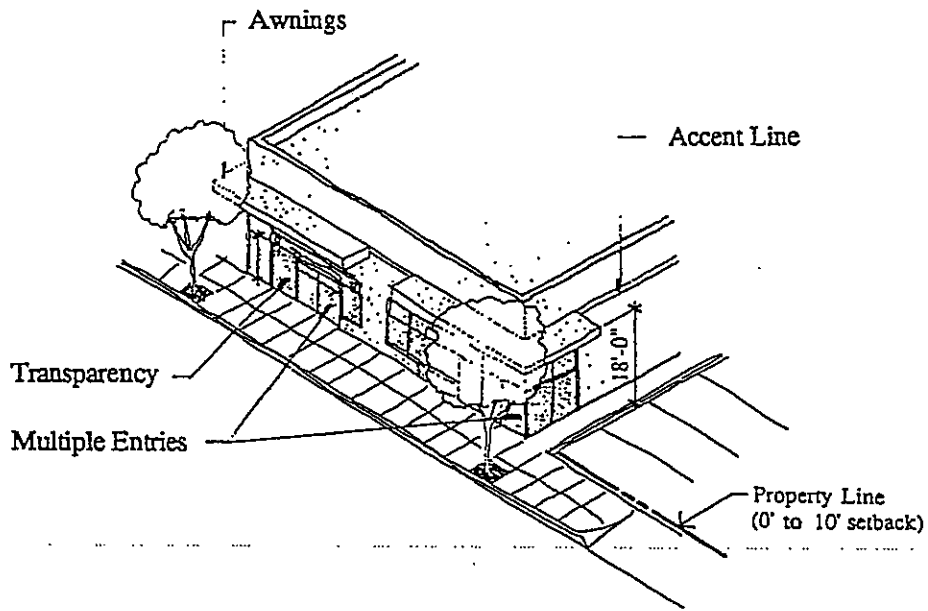
### 2. Medium Lots

The system of walls continues in this scheme for medium size lots (approximately 100'-150' frontage x 200' deep). Multi-tenant opportunities exist in this medium lot development example.

- Building wall surfaces should occur at the 10 foot setback line for a maximum linear distance of 40 feet; then a building plane offset, change in material, or other type of articulation will be required. This design guideline also establishes a clear definition of the public right of way with the use of varied wall planes and fences (See Medium Lot Sketch on opposite page).
- Visual access to the interior of the site can be accommodated through the use of low walls with open fencing above.
- Tenant identification through signage and entrance storefronts will help to break down the scale of the wall planes.
- Street-oriented entries are encouraged for medium sized lots using small lot entry design features.



Large Lots



Commercial Development

### 3. Large Lots

Larger lots (approximately 200'-250' frontage x 250' deep) can accommodate large buildings or a development complex.

- Buildings shall have a sequence of wall planes through fences and the building facade.
- Building facade surfaces should be located at the 10 foot setback line.
- They should also have linear limitations of 50 feet maximum before a building plane offset of 3' to 5' is required for a length of 25'. (See Large Lot Sketch on opposite page)
- Street-oriented entries are encouraged for large lots similar to small and medium lots.

### C. Commercially Zoned Development

The commercial zone occurs in three zones within the district. Market Street, 32nd Street and 'F' Street are the commercially zoned districts. Architectural treatment of storefronts with pedestrian scale elements are suggested in this zone. The guidelines propose enhancement of the storefront entrances and transparent segments of storefront; which will create a sense of invitation and openness. A good example of this kind of commercial development is the commercial development in Ocean Beach. There are storefronts, awnings and a strong public streetscape space.

### D. Building Heights

Building height shall be as permitted in the Southwest Planned District Ordinance and Gateway Center's C, C and R's with a maximum height of 30' - 0".

### E. Special Study Area: Commercial Center

A small commercial center is recommended for service and dining business opportunities. This center will be a central hub for workers at Gateway Center. The existing vacant building is suggested to be renovated and enhanced with the color and material scheme found on page 18 of this text. The architectural concept for renovation of the existing building will be to emphasize simple stucco facades with storefronts, awnings and signage. Deli and other small restaurant uses will be encouraged at this location. Additional discussion of this special area is within the Landscape Standards section on page 38.

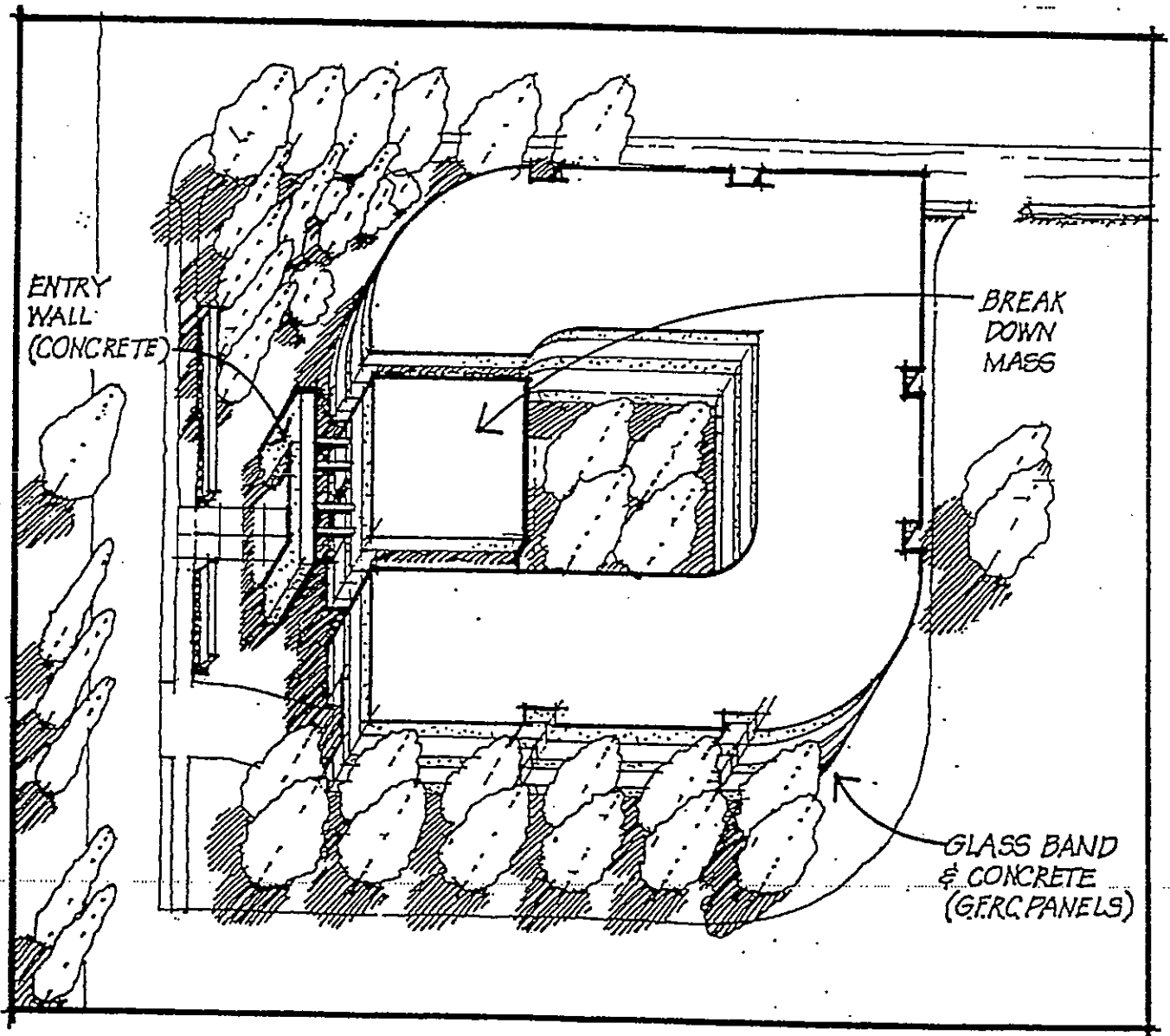
### F. Special Study Area: SDG & E Site

San Diego Gas & Electric is proposing a 20,000 square foot 2 story office building with a 10,000 square foot floor plate. The building is generally proposed to follow the outline of the existing parking lot. (See page 16) The project is currently in the schematic design phase, and construction is slated for the mid 1990s in accordance with the existing owner's participatory agreement with the Redevelopment Agency.

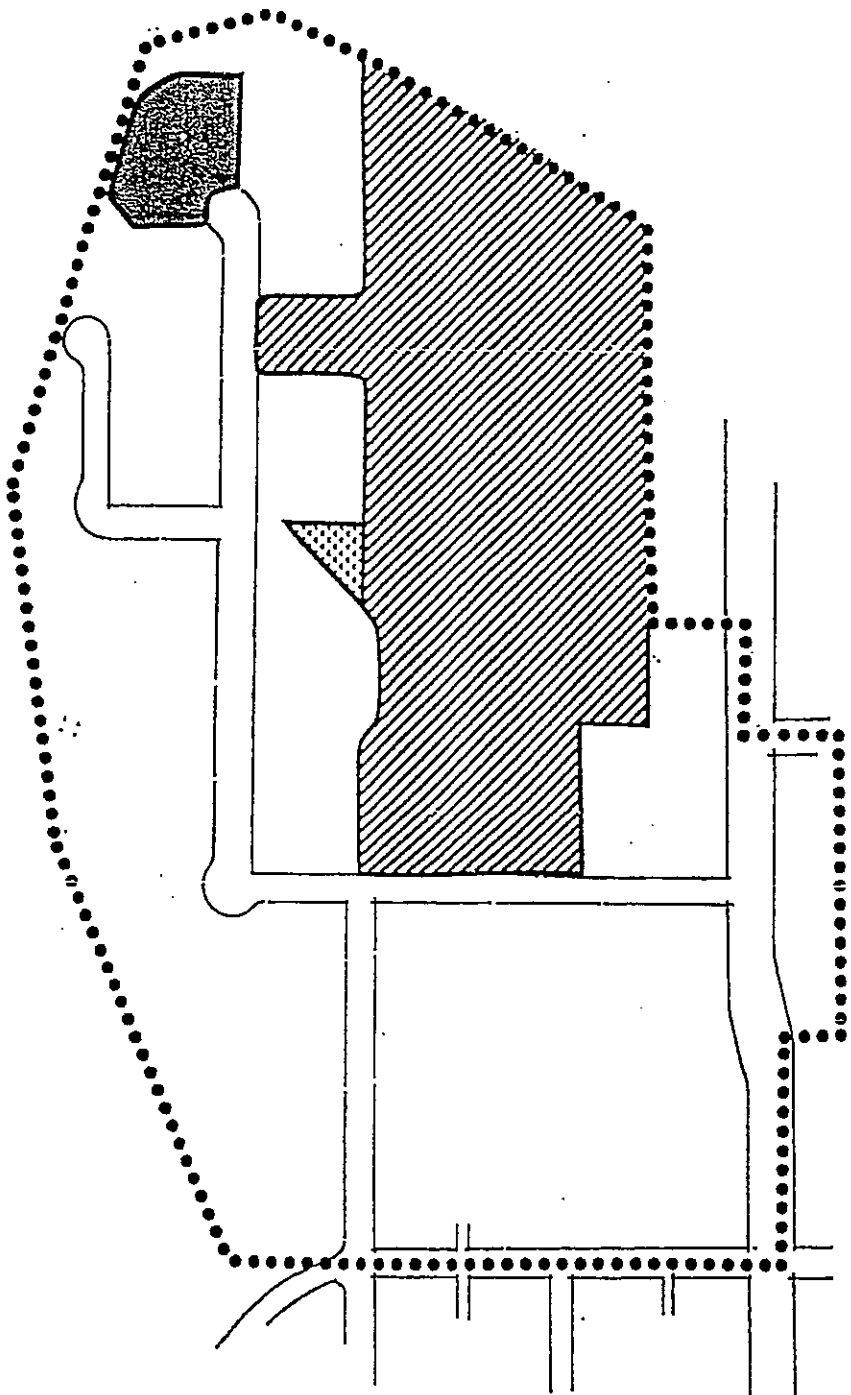
Building entrances to the office building should consider pedestrian access to 33rd Street and the north side of the site accessing the Commercial Center.

The proposed building is to have a large landscaped setback; being that this structure will be the only modern multistory office building within the district, it will be an anomaly with its own architectural character. The landscape theme for the building and the site shall comply with the landscape standards. Planning for pedestrian access to the Commercial Center along the northeast extension of 'F' Street is strongly recommended. The current extension of F Street is a driveway used exclusively by SDG&E linking Sunrise Street to 33rd Street. The driveway is gated at 33rd and Sunrise. This driveway could be opened for pedestrian access during normal working hours.




EXHIBIT NO. 7



# GATEWAY CENTER WEST



## LEGEND

-  SDG&E Study Area
-  Commercial Center Study Area
-  Archie Moore Study Area



LAND PLANNING  
ARCHITECTURE  
ENGINEERING  
SURVEYING

4377 Viewridge Avenue  
San Diego, CA 92123-1669  
(619) 218-3750  
FAX (619) 218-9758

## SPECIAL STUDY AREAS

### G. Special Study Area: Archie Moore Site

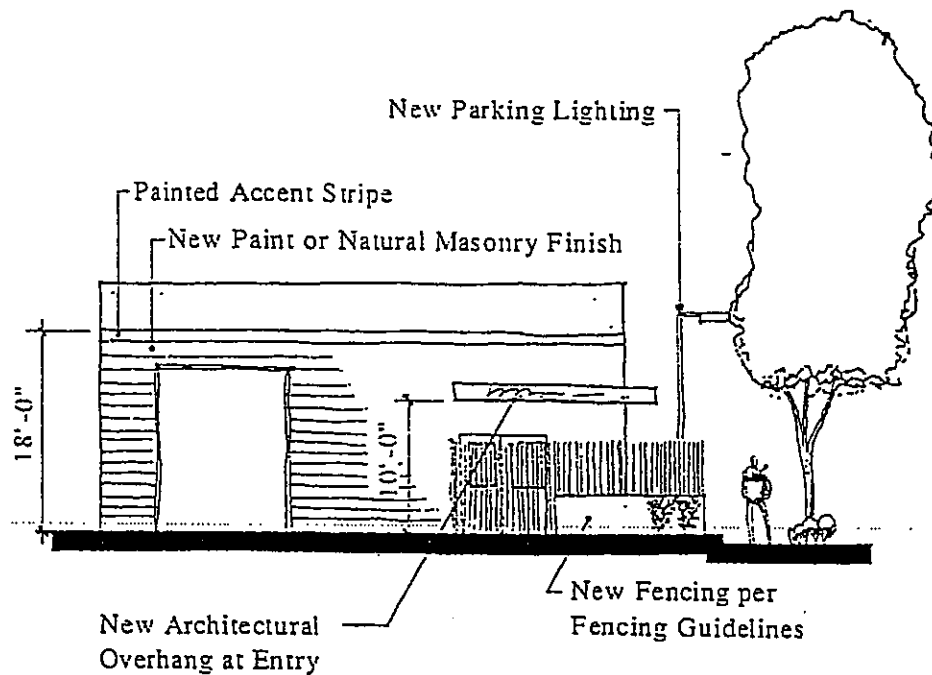
In order to anchor the end of E Street, it has been proposed that a major building such as a gymnasium be located on lot number 1 north of Archie Moore's house. This will provide the needed activity at this end of the center as well as a visual terminus to E Street.

### H. -- Renovation of Existing Buildings to Remain

Buildings that are proposed to remain within the district shall be renovated to follow the guidelines within this document.

Since the facades facing the street are the most dominant element of these existing buildings, the renovation guidelines concentrate on these. Unification of the center will be achieved through the improvement of existing facades with the use of new storefronts, new painting, awnings, site walls, the "street wall" concept and other design elements.

- Existing facades shall conform to the material and color matrix in these guidelines. Accent stripes and architectural projections shall conform to the heights as shown in the Renovation Guidelines Sketch.
- Existing non-conforming perimeter walls and fencing shall be upgraded to the new standards as specified on pages 22 and 23.
- Existing non-conforming lighting and signage should be upgraded to these guidelines and standards.



### *Renovation Guidelines*

## I. Light Standards

There are few existing light standards in the Gateway Center West development. The existing lighting is generally inadequate for pedestrian activity and for security at night. The proposed street light design standards at Gateway Center will improve security and will lend to a consistent street appearance. The new street light standards shall be spaced at 120 feet on center, staggered on each side of the street.

### 1. Street Lights

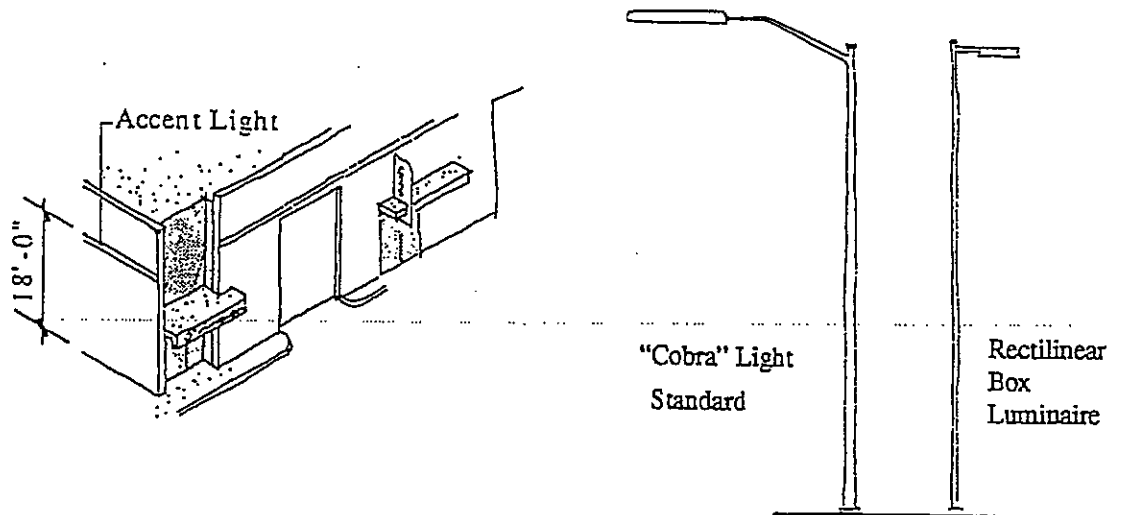
A City standard steel "Cobra" type street light fixture is proposed for street illumination. The pole shall have a galvanized finish in accordance with City of San Diego's Standard Special Provisions, Street Lighting and Traffic Signals. Provide matching base plate covers typical at each light standard.

### 2. Private Parking Area Lights

Lighting for private parking areas and drives should be provided at all new developments. Pole mounted fixtures shall be a "box type" luminaire standard on a square pole with colors as specified in the color guideline. Light standard shall be similar to Beacon Illumination Inc., "Lexington" Model.

### 3. Building Mounted Lights

Building mounted lights are required to increase security along the street frontage. A unified frontage appearance can be achieved through the use of one fixture type placed at a uniform height above ground level. Each building shall incorporate a regular spacing of light fixtures along their facade facing the street and within the parking areas. This spacing shall be consistent with the design of the building and shall provide enough light for night security. The use of an accent lights such as neon in a horizontal line at a height of 18' is encouraged.



*Lighting Guidelines*

## J. Sign Guidelines

The sign guidelines are intended to instill a simple order to business advertisement signs. Uniform signage design standards will help to eliminate much of the existing visual clutter at Gateway Center West. In addition to these guidelines, all proposed signs shall meet the City regulations for the placement and permitting of signs as identified in the City of San Diego Sign Ordinance and the SEDC PDO. The proposed sign guidelines are indicated below:

### 1. Building Mounted Signs

#### a) Industrial developments

Signs shall be attached to the building or to architectural projections such as overhangs and awnings. Horizontal building mounted signs shall start at a point 10' - 0" above ground level and be no more than 30" high and no more than 8' - 0" long. Vertical or horizontal blade type signs can be used and start at a point 10'-0" above ground level. Vertical blade type signs shall be no more than 30" wide and no greater than 6' tall.

#### b) Commercial Developments:

A uniform building sign is recommended for advertising commercial shops. A horizontal building mounted sign should be employed; the sign shall start at a point 10' - 0" above ground level and be no more 30" high and no more than 8' - 0" long. Signs should be commercially manufactured and be permanently attached to the storefront or building surface. Temporary window banner type advertising is discouraged in the commercial zone.

### 2. Ground Mounted Signs

Monument signs for advertising major complexes or a major tenant shall be in natural cast concrete and shall have a maximum height of 3'-0" and shall have no more than 30 square feet of total surface area. Ground mounted signs shall not be placed in the right-of-way nor be any closer than 3' from a parking lot or entrance drive.

### 3. Street and Directional Signage

Street and directional signs shall be in accordance with the City of San Diego standards.

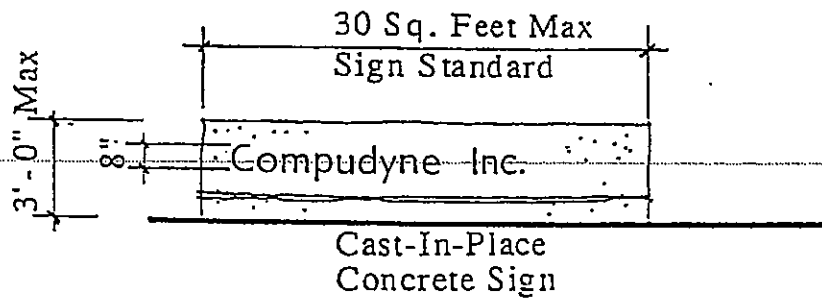
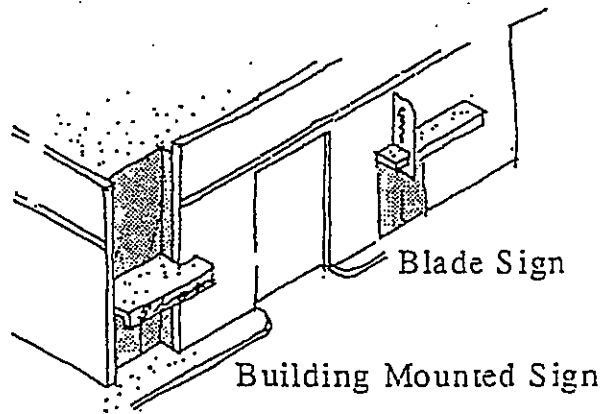
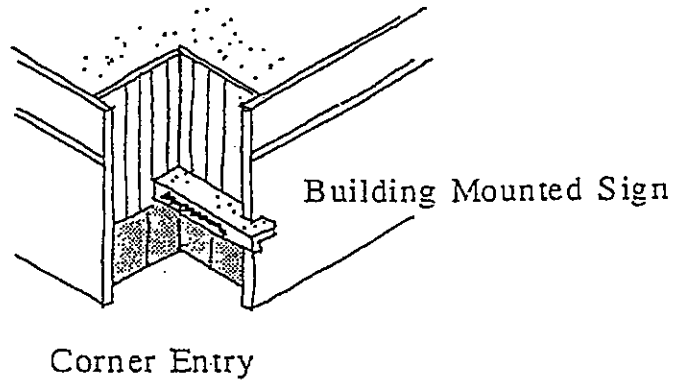
### 4. Logos and Lettering

~~Logos can be placed within the sign area and shall not be greater than 30"~~ square. Lettering material can be vinyl die-cut, screened copy, fiberglass or raised cast letters. The color scheme should be as identified in the color section.

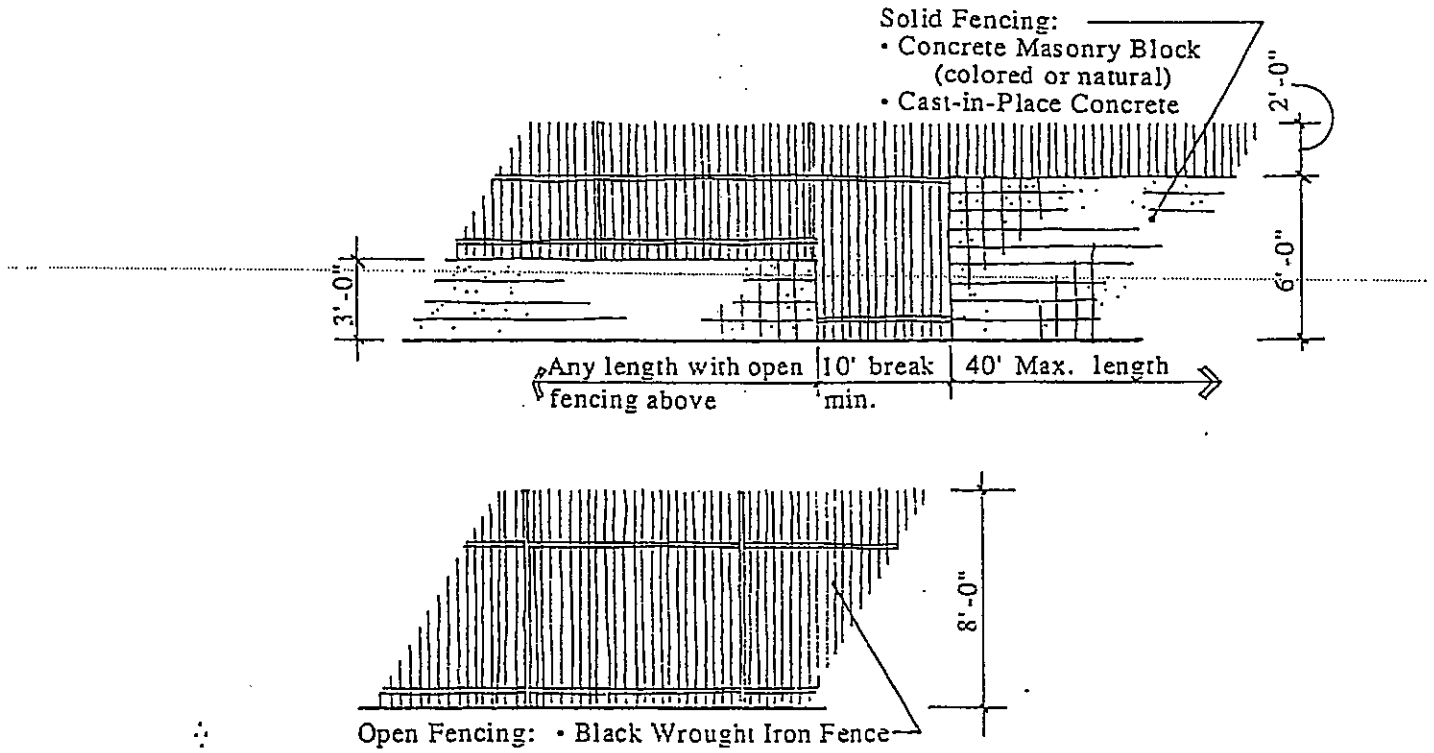
5. Materials

Generally, materials used for signs can be metal, plastic, fiberglass, fabric, neon, painted solid wood or concrete. Concrete shall be used for ground mounted signs.

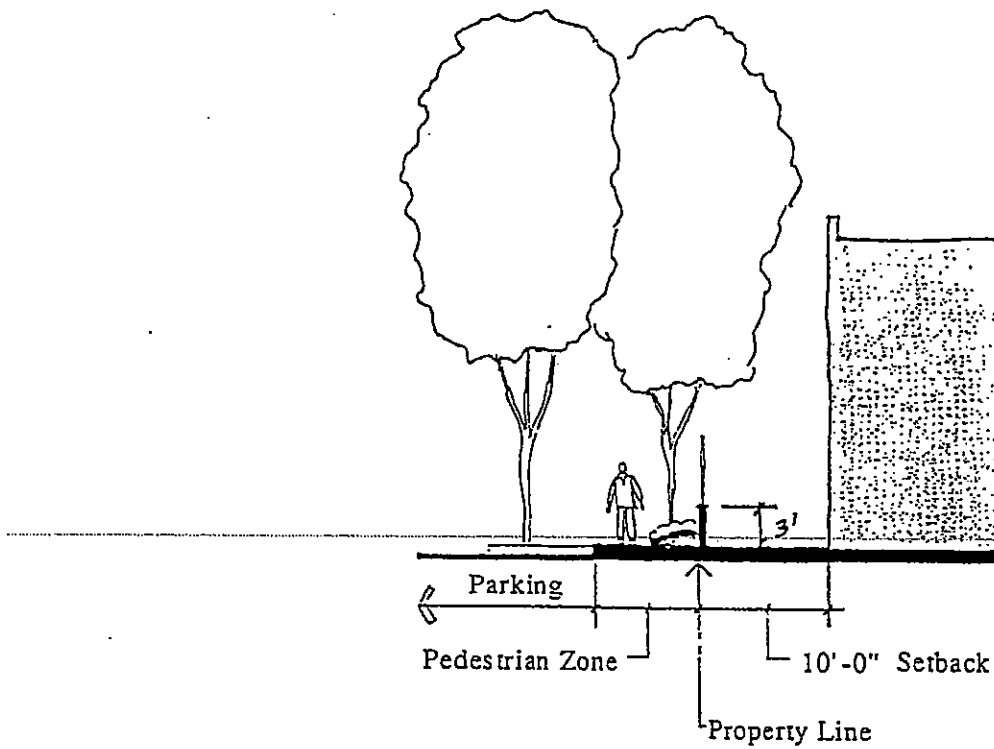
Signs shall be properly attached to the structure. The sign shall have permanence and not be made of materials such as exposed plywood, particle board or other non-permanent appearing materials. Flashing or moving signs are to be discouraged. All signs shall have approval of the SEDC prior to their design and installation.



*Signage Guidelines*



*Fencing Guidelines*



*Street Wall Definition*

## K. Security Fencing and Walls

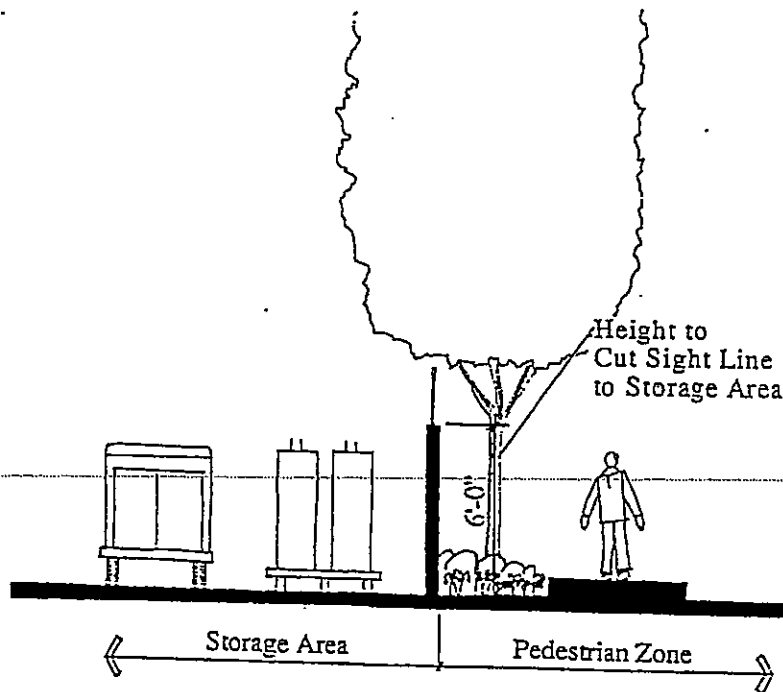
Articulation of fences shall be maintained through the use of breaks in fencing material and offsets. The areas of open fencing will allow visual access to the interior of the site. Where open fencing is desired, a black square section wrought iron fence should be installed. Creating variety and interest in wall planes is desired and to achieve this, several types of fences are suggested. As discussed in Section IV, Project Description-Concept Plan, the "street wall" concept is designed to establish a more secure environment as well as definition to the public space. The design establishes periodic openings which create a visual corridor into parking and storage areas. Materials should include a colored concrete masonry unit, cast-in-place concrete or quality metal panels. Surface relief is introduced through the use of low walls with open fencing above, breaks and surface texture. Chainlink fence and all types of barbed wire are prohibited in the district. (See Fencing Guidelines Sketch on opposite page)

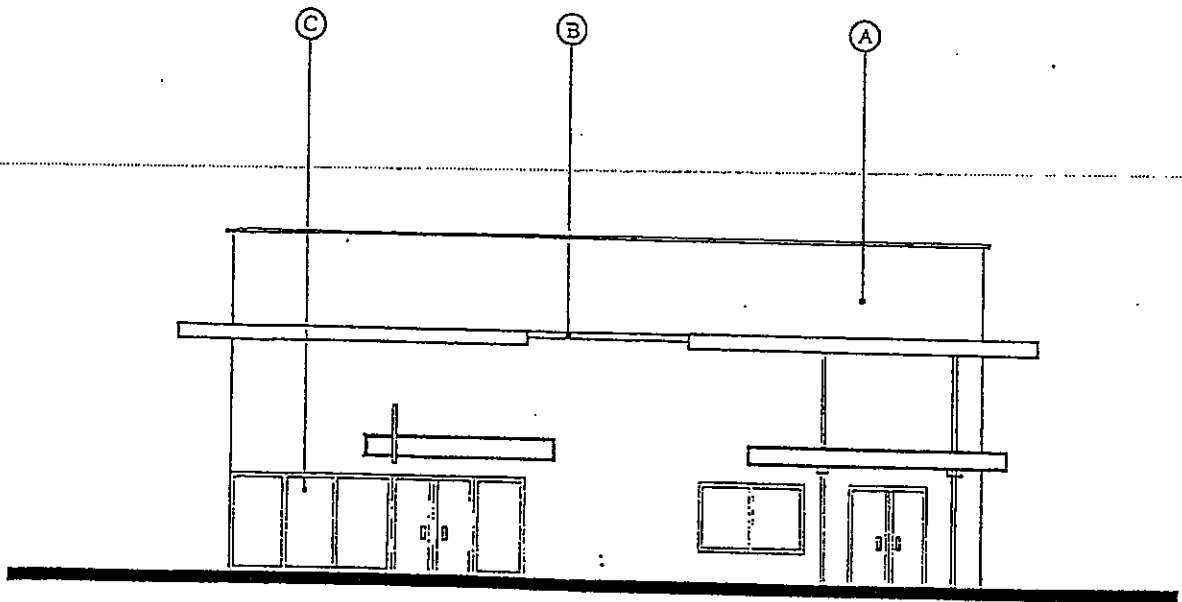
Driveway openings shall be secured with wrought iron rolling or swinging gates. These gates should comply with the same guidelines for fencing. (See Fencing Guidelines Sketch)

Low shrubs and vines are recommend to be planted near solid walls to discourage graffiti. Where plants do not occur a concrete block sealer paint should be applied to allow easier removal of graffiti.

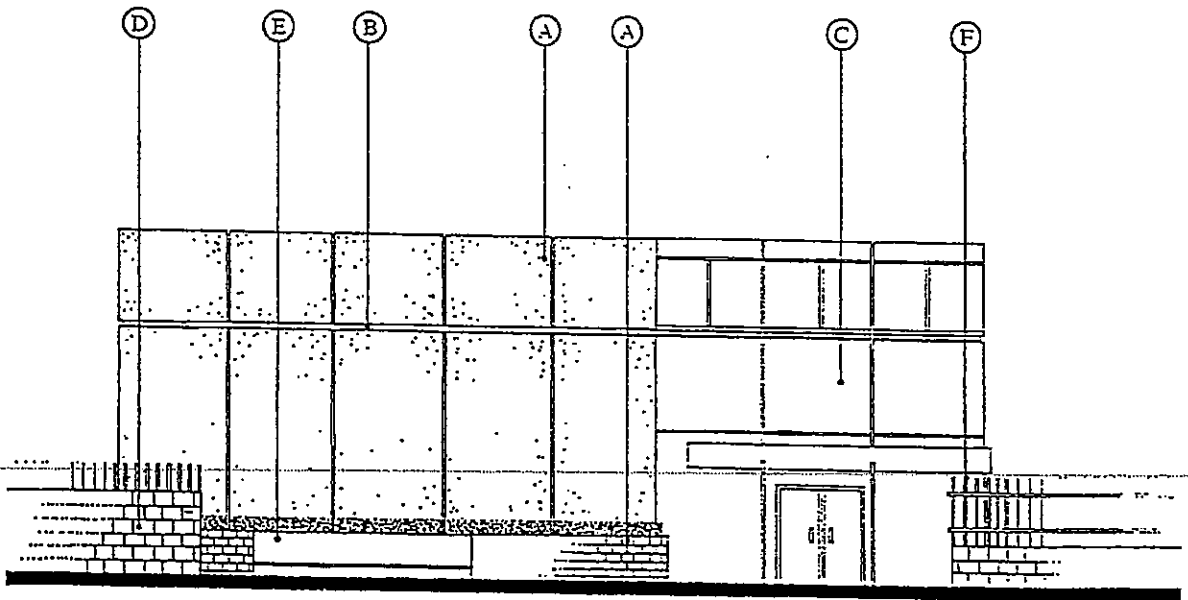
## L. Outdoor Storage

The exterior yard storage of merchandise, material or equipment be only where permitted by the PDO. The storage area shall be enclosed by solid walls or fencing. Materials shall not be stored to a height greater than the height of the perimeter walls. At sites adjacent to the freeway, outdoor storage should be confined to areas behind taller landscape. (See page 40 of the landscape guidelines)





*Color / Material Matrix for Commercial Development*



*Color / Material Matrix for Industrial Development*

## M. Color and Materials

A simple palette of materials is suggested for Gateway Center West. Exterior wall surfaces with street frontage shall have a color scheme as proposed in this section. Industrial and commercial developments are suggested to have color schemes that encourage unification while allowing unique accents for each development zone. The selection of harmonious colors for the building facades, signage and street furniture will contribute to continuity of the "streetwall" concept. The following color chart and sketches on the opposite page show the suggested color and material palette.

GATEWAY CENTER WEST COLOR AND MATERIAL MATRIX

SYMBOL	BUILDING COMPONENT	MATERIAL OPTIONS	COLOR OPTIONS
(A)	BUILDING FACADE	CONCRETE, MASONRY, STUCCO	NATURAL FINISH CONCRETE/MASONRY SAND-PRECISON, INTEGRAL COLOR MASONRY TERRACOTTA-PRECISION, INTEGRAL COLOR MASONRY CASTLE GREY-PRECISION, INTEGRAL COLOR MASONRY EXT. 396, INTEGRAL COLOR STUCCO POLAR BEAR 5420W, PAINTED SURFACE PEBBLE PINK 4380W, PAINTED SURFACE
(B)	ACCENT	PAINT	PLYMOUTH RED 4275A (INDUSTRIAL ACCENT) BLUE SLATE 5484D (COMMERCIAL ACCENT)
(C)	STOREFRONT	ANODIZED METAL, GLAZING	BLACK
(D)	WALL	CONCRETE, MASONRY	SAME COLOR PALETTE AS BUILDING FACADE
(E)	GROUND SIGN	CONCRETE	NATURAL FINISH, CONCRETE
(F)	FENCING	WROUGHT IRON	BLACK

NOTE: RCP MASONRY UNITS, EXPO STUCCO, AND FRAZEE PAINT ARE SPECIFIED FOR PURPOSES OF THIS CHART. HOWEVER, THESE COLORS CAN ALSO BE MATCHED BY OTHER MANUFACTURES OF SIMILAR QUALITY.

## VI. LANDSCAPE STANDARDS

### A. Landscape Concept

The Landscape Concept for Gateway Center West aims to give the development area a unified appearance while setting a unique image appropriate to the predominant office and light industrial uses of the site. The followings are existing site conditions which influence the development of the landscape concept:

- The site is located on a geographic "saddle," or in a valley. The north and south sides of the site are open, while the east and west sides rise to hilltops. The east and west views, therefore, are oriented towards landforms while the north and south views are oriented towards the sky.
- The existing development pattern follows an orthogonal north-south-east-west grid, establishing potential north-south, east-west access corridors with very different views.
- The site is framed on two sides by the curvilinear edge of freeways, I-94 to the north, I-15 to the south.

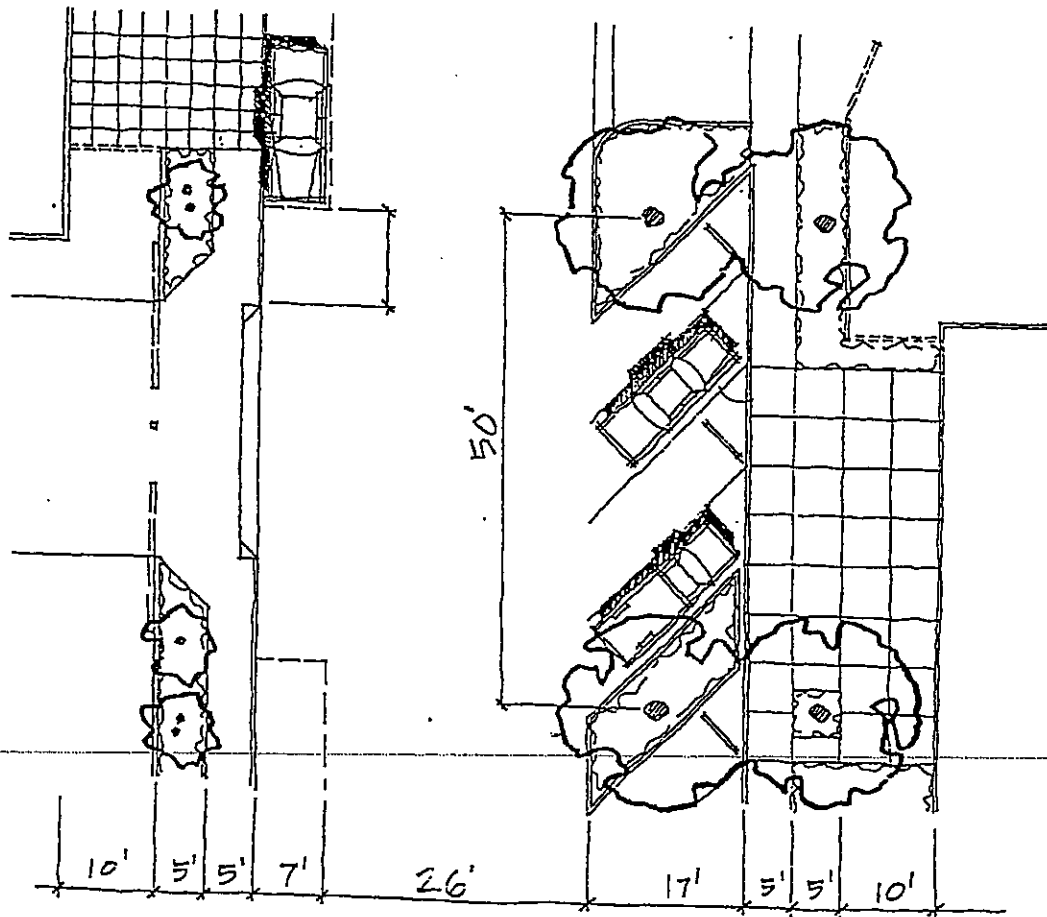
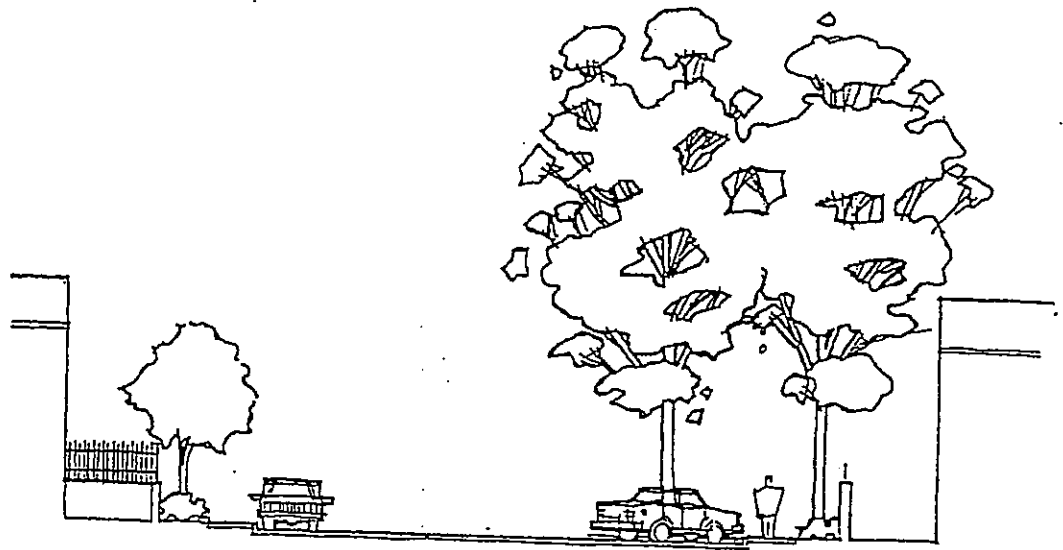
In response to the above conditions, the landscape concept proposes four fundamental goals to address the large scale aspects as well as the personal aspects of the landscape:

1. to reinforce the orthogonal character of the site;
2. to strengthen the site's sense of boundary, particularly along the freeways;
3. to focus views to the "near" landforms, namely the hilltop of Gateway Center East; and,
4. to serve users at the pedestrian scale, providing color, screening, and micro-climate control where necessary.

The placement and selection of plant material for Gateway Center West has been done with these considerations in mind.

### B. East-West Corridors

The term "corridor" applies to streets, alleys and pedestrian ways that, by virtue of their being framed by buildings, walls, or vegetation, offer views in a predominant direction. In Gateway Center West, the main east-west corridors are E, F, G, and Market Streets; the alleys between these streets; and the existing SDG&E buildings, which have long facades along an east-west orientation. These corridors are longer than the north-south corridors on the site. They also focus views on the Gateway Center East hilltop. To reinforce the views of the hill and to strengthen their geographic orientation, double and single rows of Eucalyptus are proposed along the site's east-west corridors. From surrounding streets, the freeways, and other distant vantage points, the proposed Eucalyptus trees will give the site an unmistakable visual signature unlike any other in the area. A stout, large Eucalyptus specie should be used, such as the *Eucalyptus cladocalix* and *Eucalyptus citriodora*. While the corridors share the same tree species, each is conceived differently according to their specific width and pedestrian-scale requirements (see axonometric view exhibit on page 10).



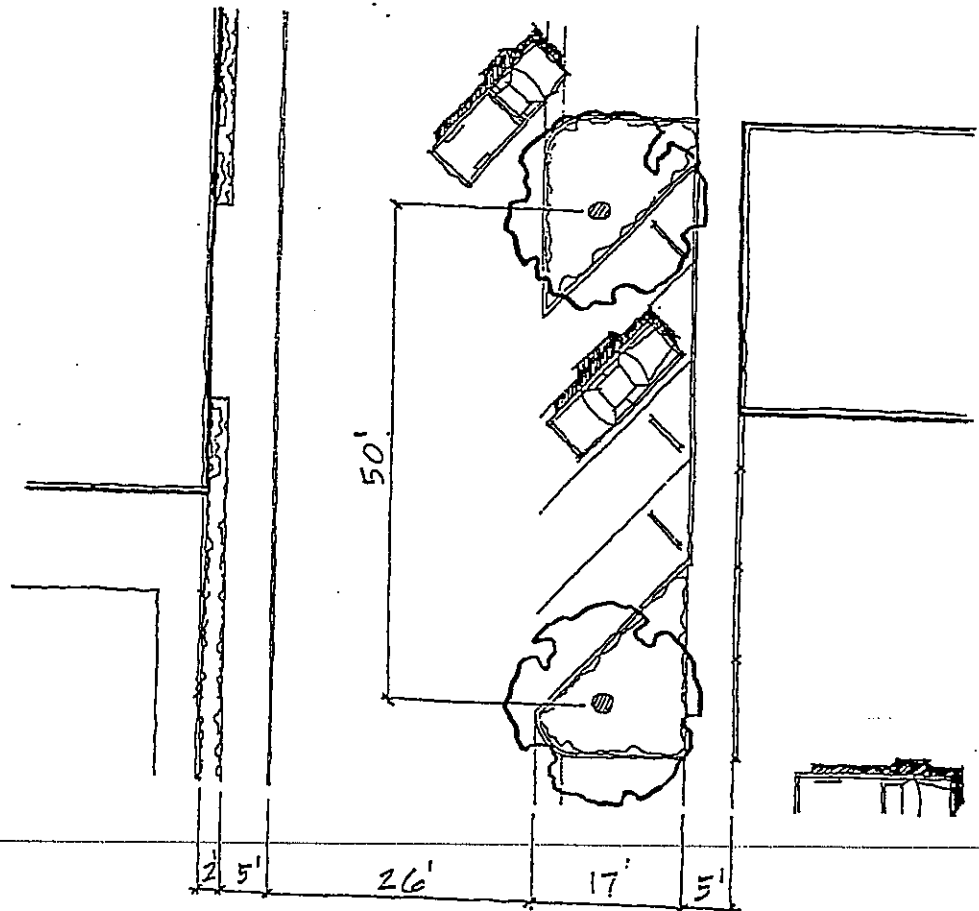
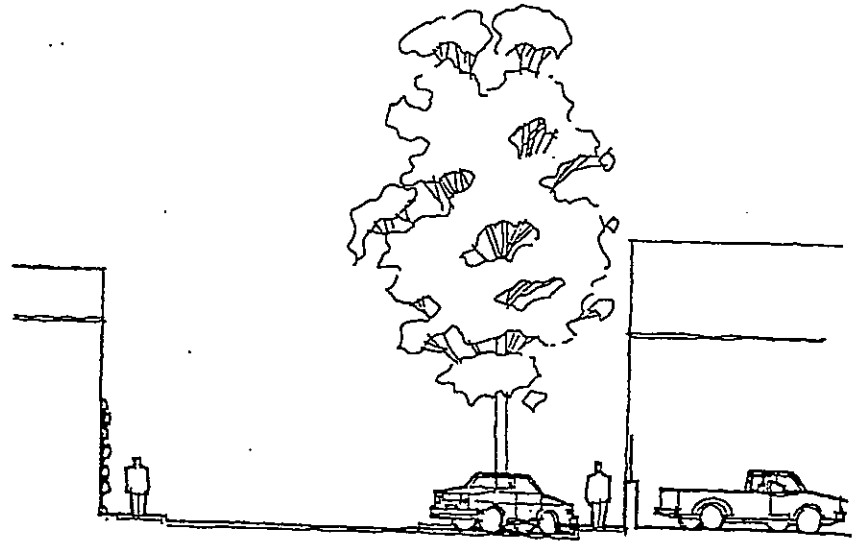
E & F Street

1. E & F Streets

One of the objectives of the plan for Gateway Center West is to enhance its character as a pedestrian-oriented place. To this end, E and F Streets, which serve a large number of commercial/industrial businesses are proposed to have on-street parking and shaded sidewalks throughout their length. These features will give the street an intimate scale with a higher degree of amenity than is common in commercial/industrial areas. A double row of Eucalyptus trees is proposed on the south side of each street, with each pair spaced about fifty feet, matching the original lot division. Four angled parking spaces can be accommodated along the street between the trees. The trees should be separated by approximately sixteen feet from each other and the main sidewalk should run between them (see E & F sketch on opposite page).

Smaller flowering trees such as the Silk Tree are proposed on the north side of each street, which also accommodates parallel on-street parking. These trees would be spaced about forty feet on center, or about every other two parking bays.

The spacing of the trees on both sides of the street should be taken into account when planning for access and service driveways. The objective should be to minimize potential conflict between the regular rhythm of the trees and the placement of the driveways.



Pickwick Avenue

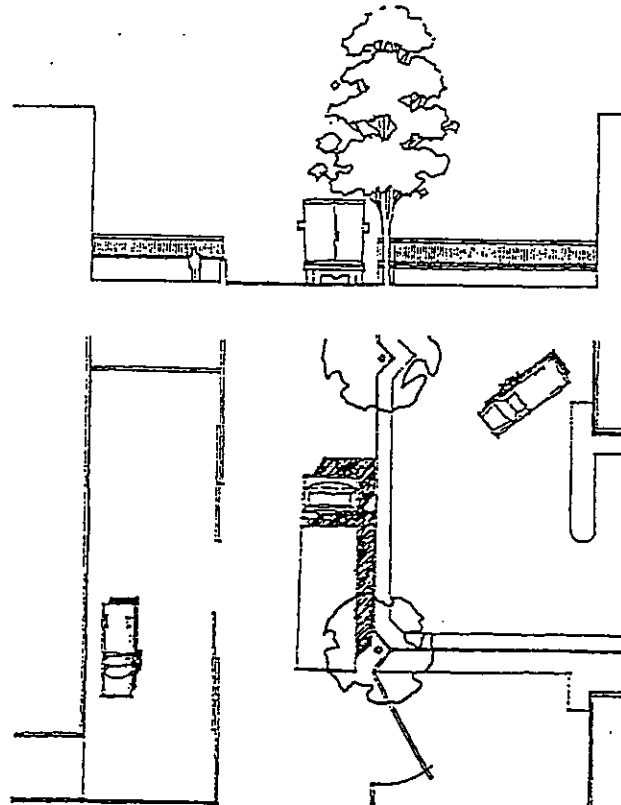
2. Pickwick Avenue

Because of its narrower width, the east-west portion of Pickwick Avenue is proposed to have a single row of Eucalyptus on its south side, spaced the same fifty feet as E and F Streets. On-street, angled parking is proposed between the trees on the south side. No parking or additional trees are possible on the north side of the avenue because of limited width. However, to add color to the corridor and to soften the appearance of the buildings, a continuous vine planting strip is proposed between the sidewalks and the building line on the avenue's north side. Bougainvillea and Japanese Honeysuckle should be considered. The planting strip should be a minimum of two feet in width (see Pickwick Avenue sketch on opposite page).

A single row of Eucalyptus should be planted east of the proposed cul-de-sac, extending to Caltrans right-of-way. The spacing of the trees should be fifty feet.

3. Alleys

The Plan proposes to retain the existing alleys north and south of F Street. Because of their limited width, the Eucalyptus rows should be placed in-line with the property lines on either side of the alleys, depending on the availability of space for planting. To make room for the trees, the fence lines could either be stepped back into the property or the trees could be planted behind fence. This recommendation minimizes the potential for the trees to obstruct vehicular movement along the alleys. The spacing of the trees should remain fifty feet on center along either side of the corridor.



*Alleys*

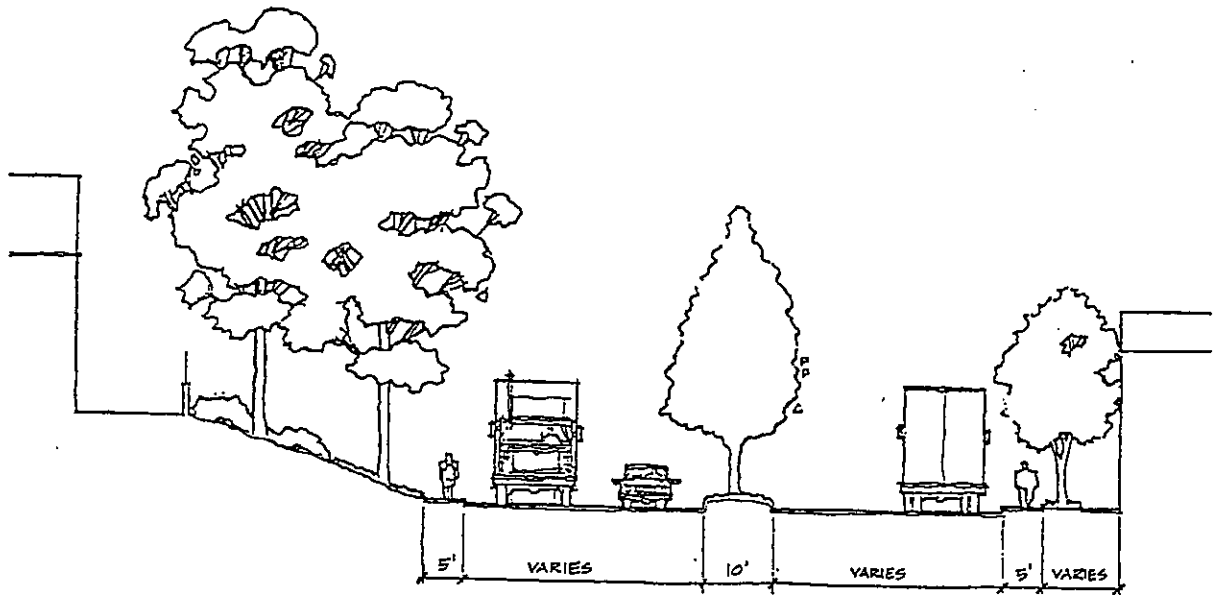
4. Market Street

A double row of Eucalyptus is proposed on the north side of the street to link Market Street to the overall image of the project. The trees would be planted in the buffer or setback zone between the right-of-way line and the buildings. A foundation of flowering shrubs and ground cover should be planted in the buffer area. Water-conserving shrub and ground cover species such as Acacia, Ceanothus, Manzanita, Lantana and Gazania should be considered (see Market Street section below).

Market Street functions as a major arterial into the area from the city's downtown. To enhance the street and mark its significance, a landscaped median is proposed down its length from 32nd Street to I-15. Carrotwood trees, the dominant street tree along Market Street should be planted thirty feet on center on the south side of the street. The median and south side street planting should be consistent in character with that in the Gateway Center East portion of the street. This would visually tie both projects at the larger, urban scale of the City.

5. Special Study Area: SDG&E Site

A double row of Eucalyptus in the form of "windrows" are proposed within the SDG & E property in an effort to maintain the continuity of the landscape across their parking area so as to mimic the traditional street pattern. Placing the trees along the building frontages would yield an image similar to that on E and F Streets, while mitigating the property's expansive appearance. The axonometric view exhibit on page 10 illustrates the concept of planting within SDG&E. Internal parking lot design within SDG&E should emphasize the east-west character of the project area.



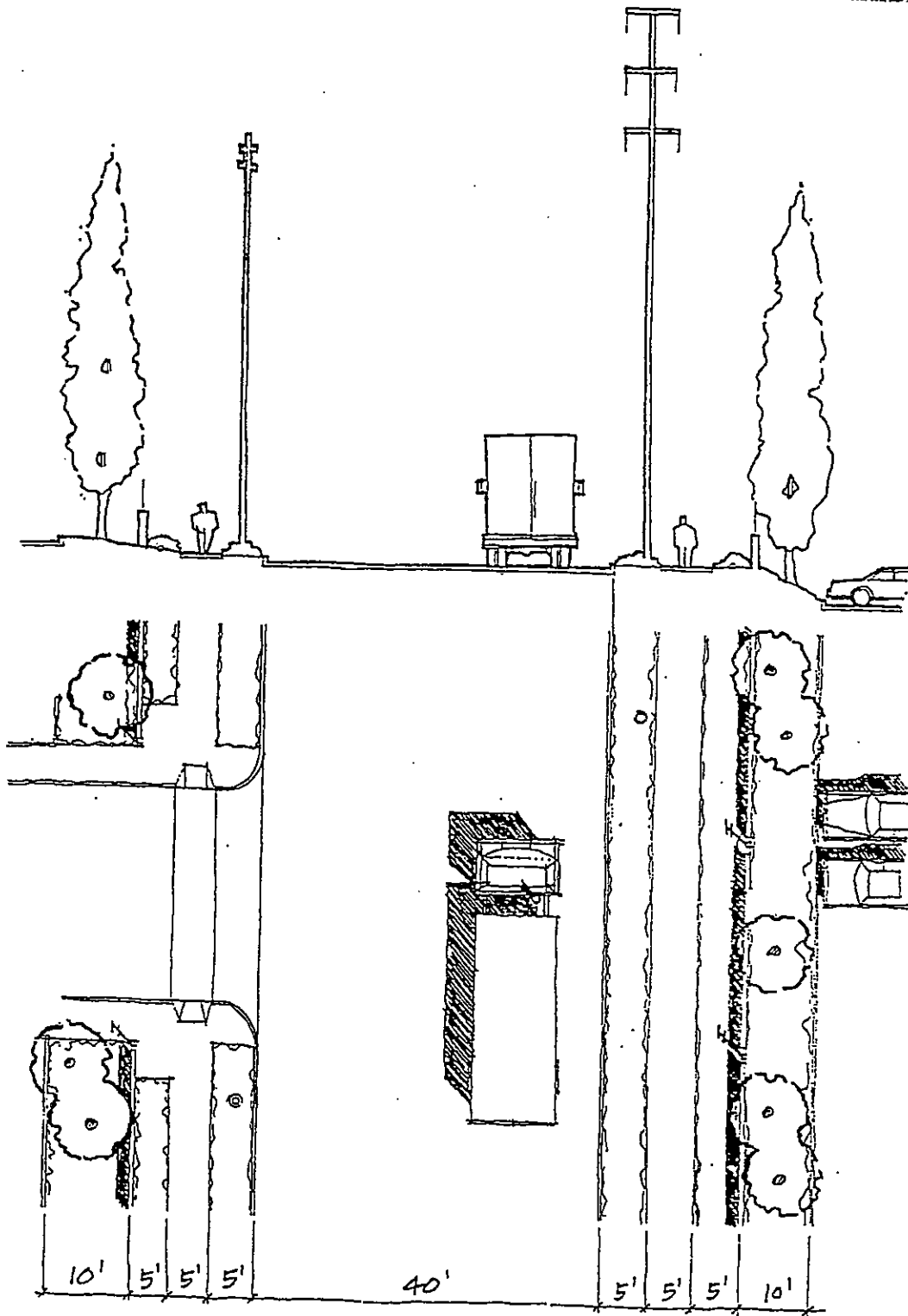
*Market Street*

## C. North-South Corridors

The north-south corridors, namely 32nd and 33rd Streets and the north-south portion of Pickwick Avenue, are conceived as a counterpoint in the landscape to the east-west corridors. Owing to their encumbrance by power lines and by the small yard areas available for vegetation, these corridors are proposed to be planted with Lombardy Poplars and Queen Palms. These slender, medium height tree plantings are to avoid any possible canopy conflict with power lines. A six foot minimum setback from the power line pole to the tree trunk is required. The trees should be planted within private yard areas, opposite the building facades wherever possible so as to minimally obscure the entrance driveways. Root barriers should be used with the Poplars.

### 1. 33rd Street & Pickwick Avenue

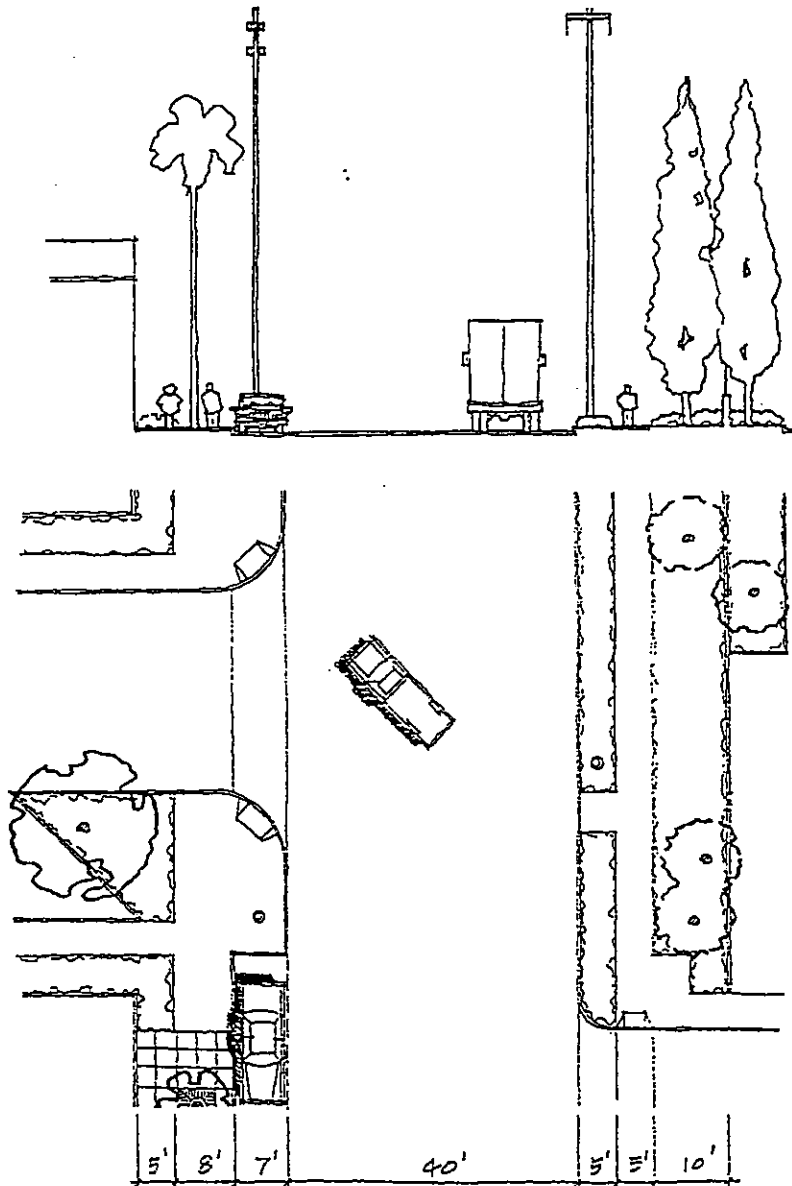
The Poplars should be planted on both sides of the street, behind any existing security fences. This minimizes potential conflict with the overhead transmission lines, which are placed in the right-of-way. To add color to the streets, continuous strips of groundcover are proposed between the sidewalk edges and the street curb lines on one side, and between the sidewalk and the security fences on the other. The aesthetic emphasis is therefore placed on the ground planting rather than on tree foliage, as with the east-west corridors. Trees would be planted in pairs or singularly, spaced about thirty feet on center. Ground covers such as Lantana, Currant, and Prostrate Rosemary are appropriate. No on-street parking is proposed on 33rd Street (see section and plan view on next page).



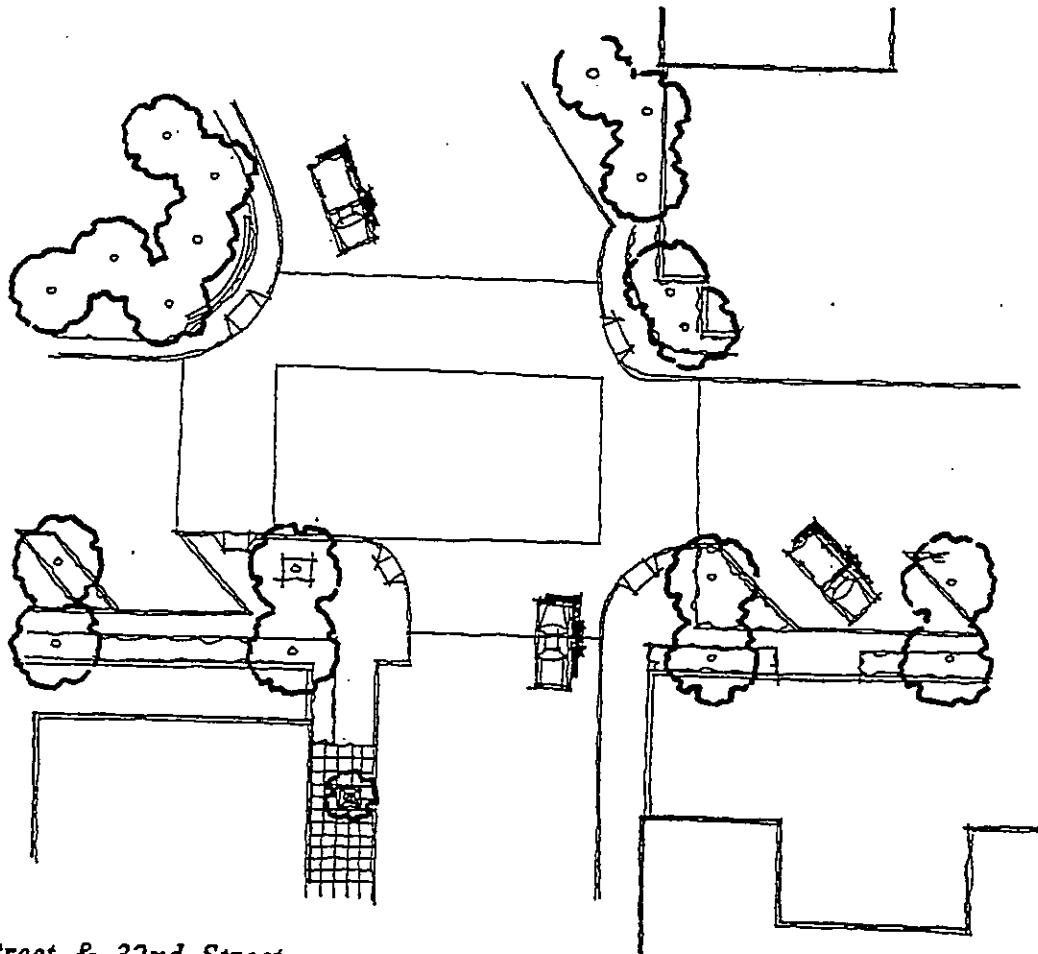
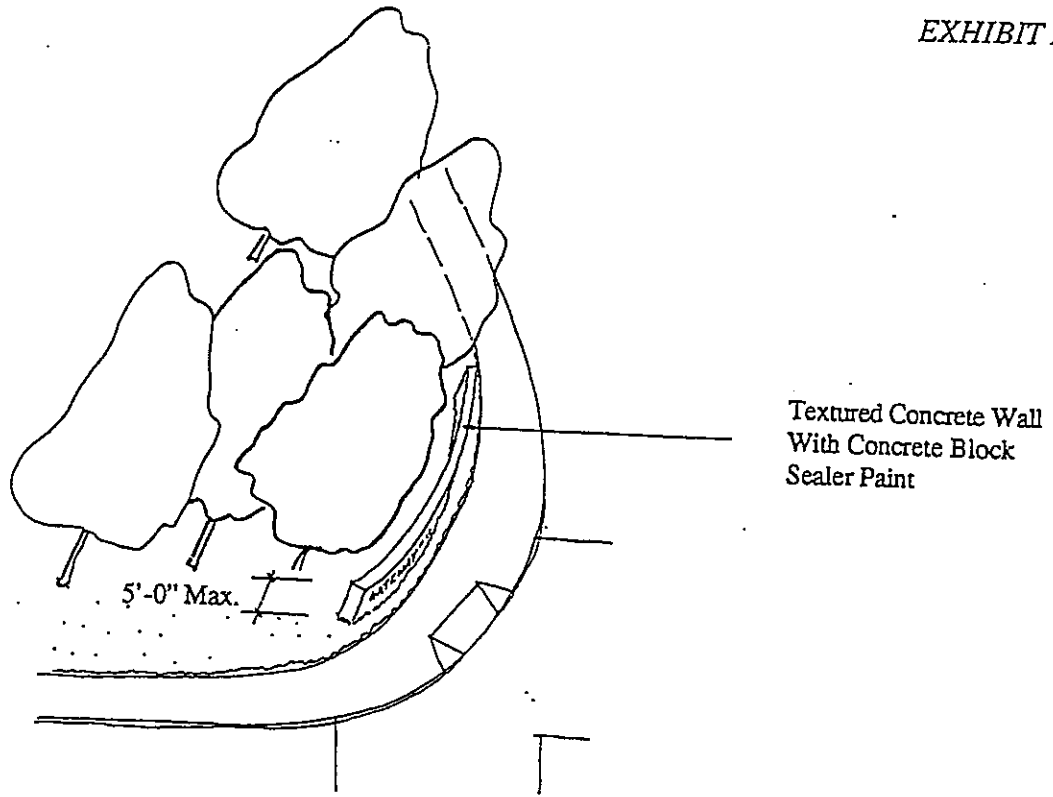
33rd Street & Pickwick Avenue

2. 32nd Street

32nd Street is the "seam" between Gateway Center West and the residential area to the west. To mark the difference between both sides of the street, a row of Queen Palms is proposed on the west side of the street, and Lombardy Poplars on the east side. The spacing of the both trees should be fifty feet on center along either side of the street. The palms should be planted on the sidewalk area in brick-covered planters to facilitates pedestrian circulation. On-street parking is possible on the west side of the street. So as to allow pedestrian use of the sidewalk, no ground cover planting is proposed on this side of the street.



32nd Street



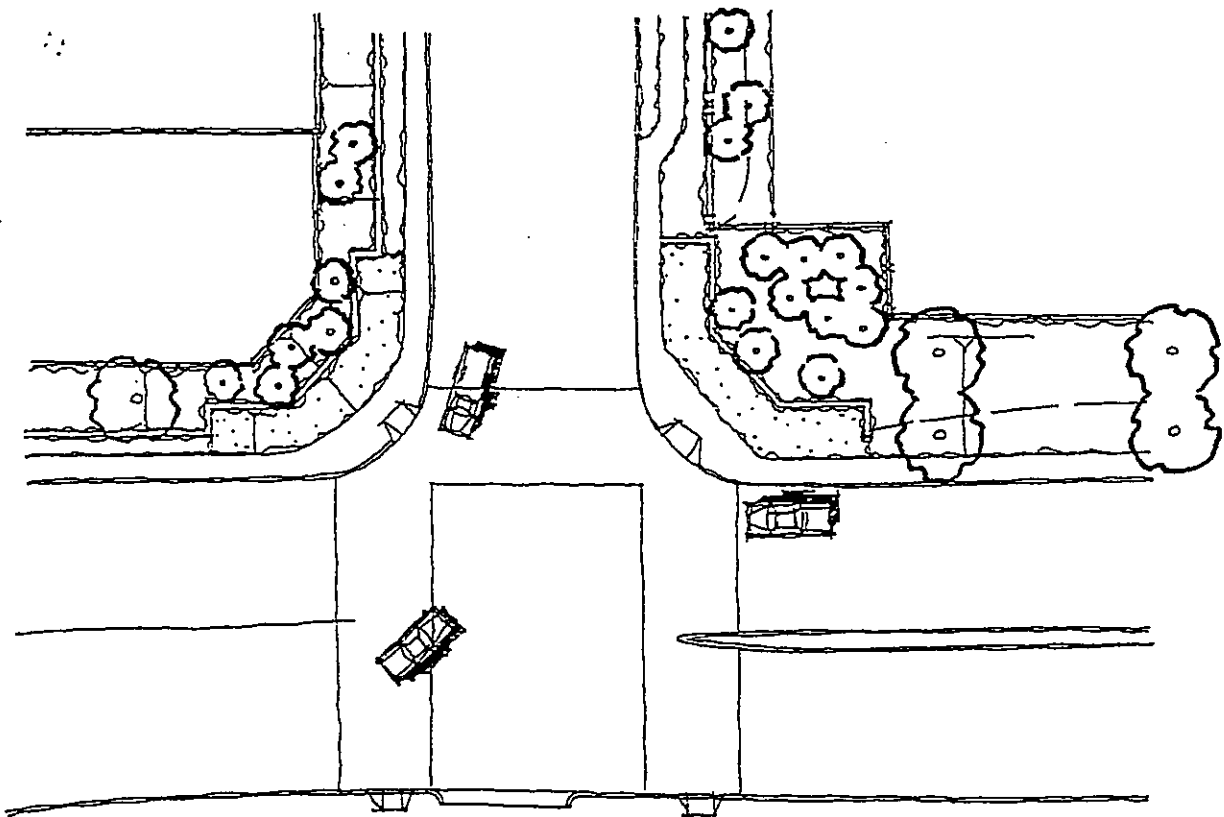
*F Street & 32nd Street*

#### D. Intersection Treatment

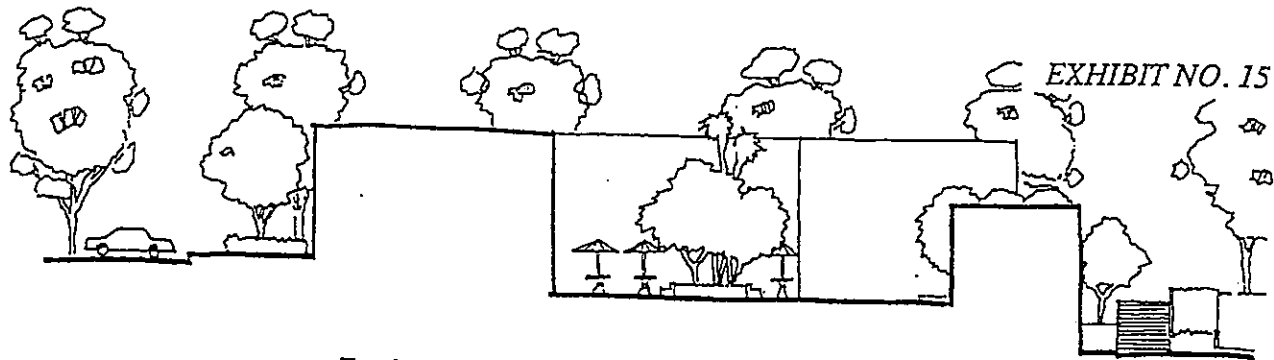
Access to Gateway Center West occurs along Market and 32nd Streets. Enhancing the entrances to the site adds to the overall identity of the place as a distinctive commercial/industrial enclave in the area. To this end, special treatment is proposed for the project's two intersections with surrounding streets: Market and 32nd Streets and 33rd and F Streets. At these intersections, special crosswalk paving (scored, integral colored concrete) is proposed as a means to mark the project entrances. (See sketches on opposite page and below) Because Market Street is the more prominent entrance, the same type of concrete wall as on the west side is recommended on the east side of 33rd Street.

The intersection of Market Street and 32nd Street is considered secondary entry point into Gateway Center West. At this intersection special crosswalk paving similar to Market and 33rd Street is recommended.

Masses of Eucalyptus trees should be planted with ground mounted "Gateway Center West" signage at the east side of F Street and 32nd Street.

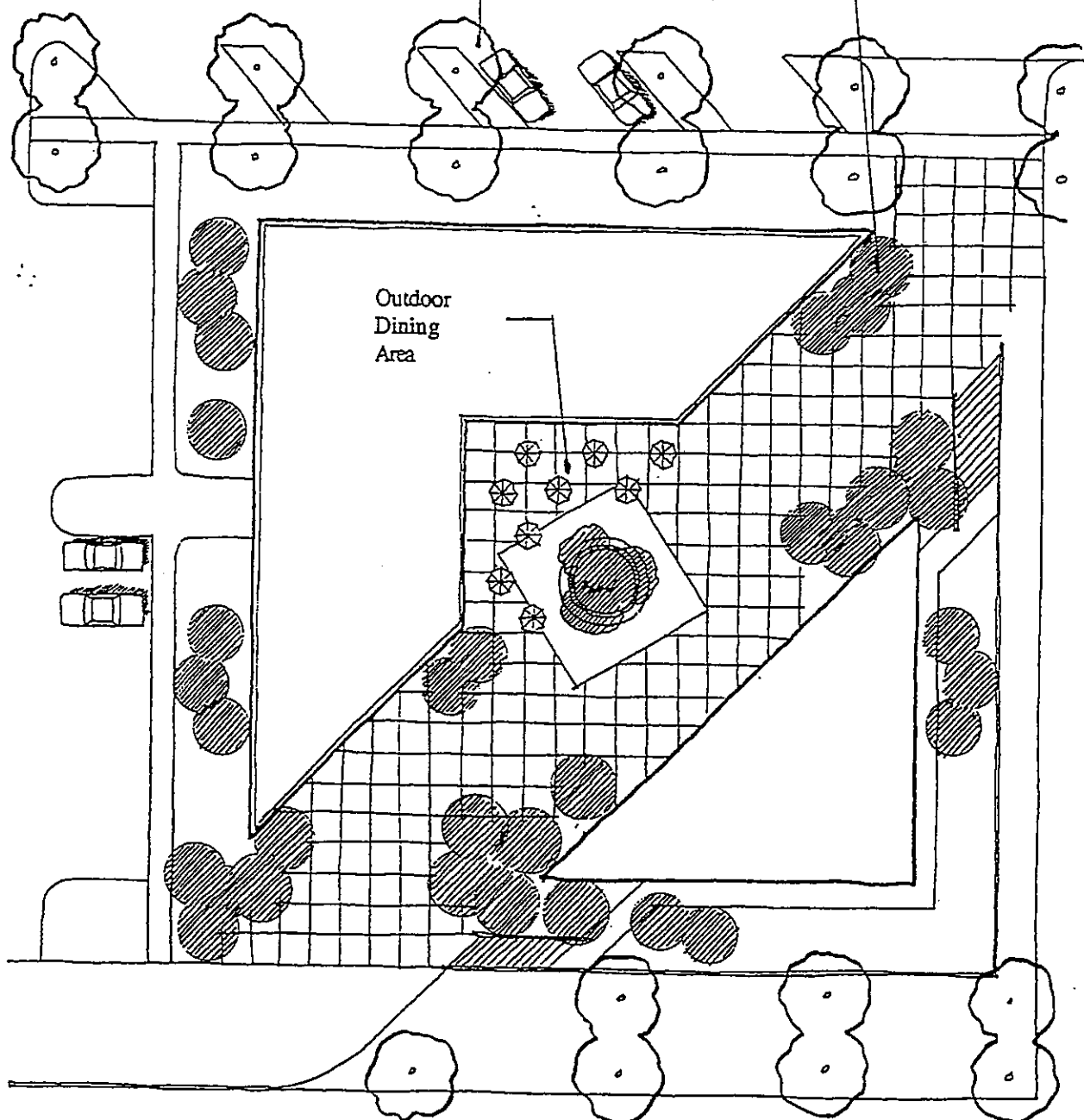


*Market Street & 33rd Street*



Eucalyptus Trees

Flowering Trees



*Sunrise Street  
Pedstrian Corridor*

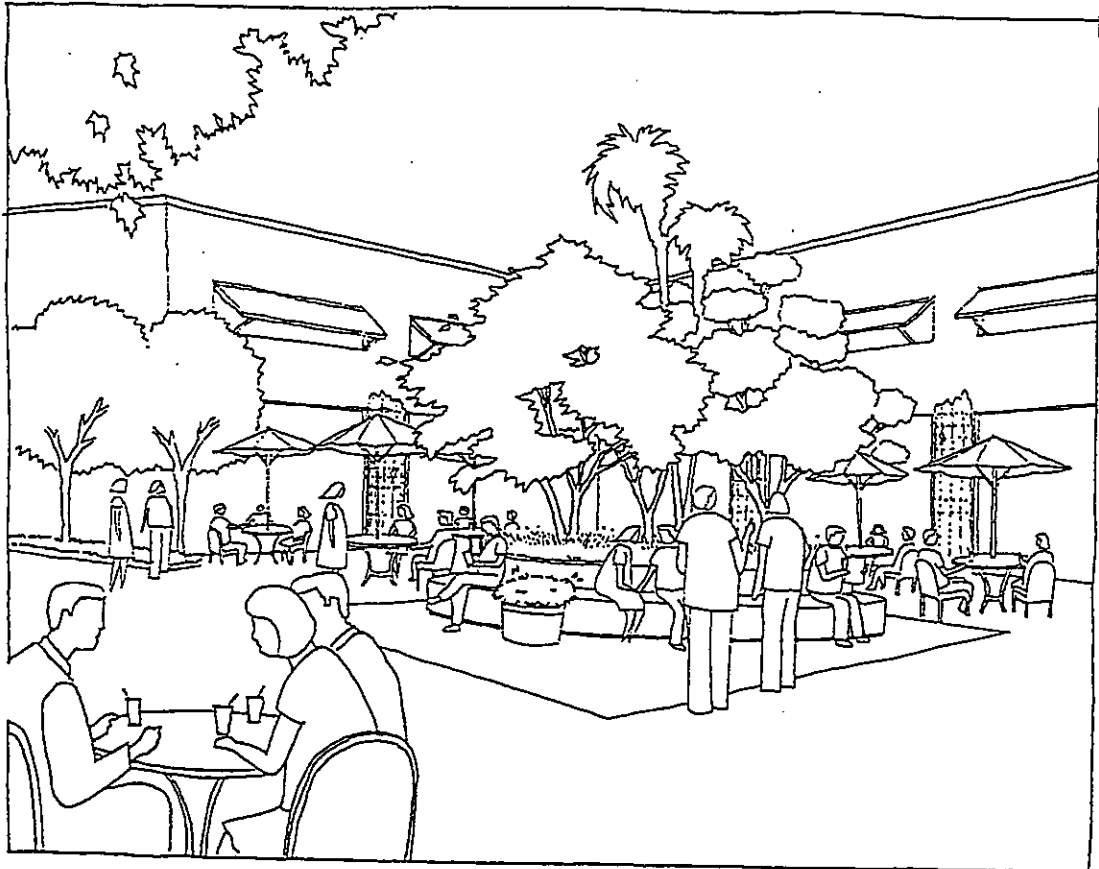
E. **Special Study Area: Commercial Center**

Sunrise Street is intended to connect F Street and 33rd Street, and E Street and 34th Street as a major pedestrian corridor, while accommodating emergency vehicle circulation. A minimum width of sixteen feet for fire truck lane is required. The landscape features of the diagonal corridor should be unique in landscape form, creating a place for rest and relaxation.

**Gathering Area:** Located at the center of the diagonal corridor, corresponding to the Sunrise Street right-of-way, the gathering area is intended to function as a major pedestrian activity area, providing enclosure, shade and a variety of seating arrangements to facilitate outdoor dining for employees of the businesses within Gateway Center West. The size of gathering area should not be less than 1600 square feet.

Planting should be informally arranged along the building with massings of trees with a limited numbers of shrubs or groundcovers. A visually prominent raised planter with a variety of flowering trees and palm trees should be located at the center of the corridor to provide a focal point. The planter should be suitable for seating.

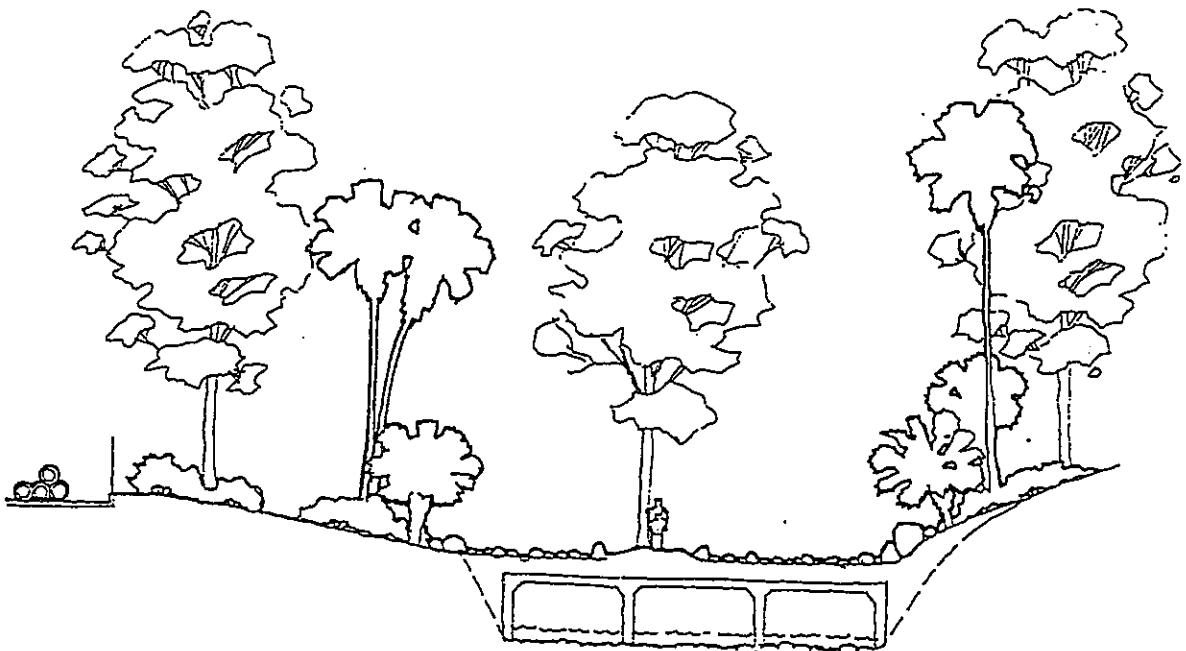
Paving should be the principal unifying element of the corridor. It should be modular, scored or patterned on a diagonal orientation from the adjacent building lines.



*Gathering Area*



*I - 94 Boundary*



*I - 15 Boundary*

## F. Freeway Boundary

Gateway Center West is highly visible from Interstates 15 and 94. Much of the vegetation currently in view from the freeways, particularly from I-94, is on Caltrans property. A joint effort between Caltrans and SEDC should be initiated to improve these areas with additional drought-resistant vegetation.

### 1. I-94 Boundary

*Eucalyptus cladocalyx* trees should be considered due to its spectacular skyline silhouette character, but they should be clearly different in form and character from those proposed for the east-west corridors. They should also be planted in naturalistic drifts as opposed to regular patterns. The wide spreading shrubs and groundcovers with deep rooting habits should be planted on slope areas. Manzanita, Acacia, Baccharis and Gazania should be considered (see typical cross-section on opposite page).

### 2. I-15 Boundary

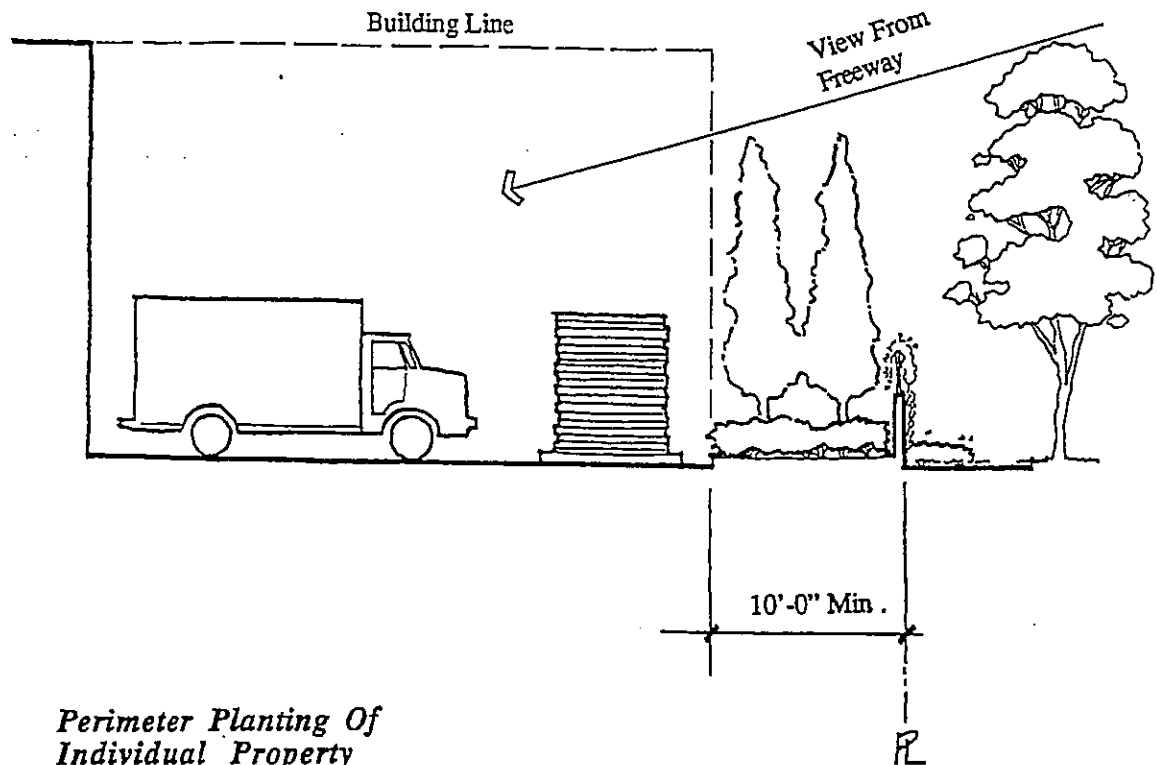
Substantially more land is available for landscape improvement along the I-15 boundary. In addition, Caltrans has proposed to cover the drainage channel running parallel to the freeway, removing a current safety hazard. This offers the opportunity to integrate the Caltrans right-of-way improvements with those on Gateway Center West. To this end, the landscape concept proposes to create a "dry river-bed" landscape over the Caltrans box-culvert, with riparian-type vegetation at either side. Species such as the California Sycamore should, therefore, be predominant. Phoenix palms along with native riparian sub-canopy shrubs should also be considered.

This area should be considered as a project-wide amenity. Places for seating and having lunch could be developed, particularly on the southern end of the boundary as it meets the SDG&E property (see typical cross-section on opposite page).

**G. Landscape Treatment of Individual Property**

Landscaping within individual property should be considered to enhance the identity of Gateway Center West and to improve its environmental quality. In particular, perimeter planting will contribute to the visual improvement from the freeways.

The Lombardy Poplars should be planted, behind security fences in rear yard setback access. The trees should be planted very densely to screen loading facilities or outdoor storage to be seen. Low shrubs and vines such as Acacia, Manzanita, Bougainvillea, and Japanese Honeysuckle should be considered along the fence. The location of outdoor storage and its required screening landscape should be carefully considered.



*Perimeter Planting Of Individual Property*

## H. Proposed Plant Materials

Street Name	Proposed Plant Materials	Remarks	
E&F St.	Trees:		
	<i>Eucalyptus citriodora</i> ,	Lemon-Scented Gum	50' O.C.
	<i>Albizia julibrissin</i> ,	Silk Tree	40' O.C.
	Groundcovers:		
	<i>Lantana montevidensis</i> ,	Trailing Lantana	
	<i>Rosmarinus o. 'Prostratus'</i> ,	Dwarf Rosemary	
	<i>Vinca major</i> ,	Bigleaf Periwinkle	
Pickwick Ave.	Trees:		
	<i>Eucalyptus citriodora</i> ,	Lemon-Scented Gum	50' O.C.
	Vines:		
	<i>Bougainvillea</i> ,	Bougainvillea 'San Diego Red'	
	<i>Lonicera japonica</i> ,	Japanese Honeysuckle	
Alleys	Trees:		
	<i>Eucalyptus citriodora</i> ,	Lemon-Scented Gum	50' O.C.
Market St.	Trees:		
	<i>Eucalyptus citriodora</i> ,	Lemon-Scented Gum	50' O.C.
	<i>Pinus canariensis</i> ,	Canary Island Pine	40' O.C. at median
	<i>Cupaniopsis anacardioides</i> ,	Carrotwood	30' O.C.
	Shrubs:		
	<i>Acacia redolens</i> ,	Prostrate Acacia	
	<i>Arctostaphylos densiflora</i> ,	Manzanita	
Groundcover:			
	<i>Gazania splendens</i> ,	Mixed Color Gazania	
	<i>Lantana montevidensis</i> ,	Trailing Lantana	
	<i>Centaurea cineraria</i> ,	Dusty Miller	
33rd St.	Trees:		
	<i>Populus nigra 'Italica'</i> ,	Lombardy Poplar	30' O.C.
	Groundcover:		
	<i>Lantana montevidensis</i> ,	Trailing Lantana	
	<i>Rosmarinus o. 'Prostratus'</i> ,	Dwarf Rosemary	

Street Name	Proposed Plant Materials	Remarks	
32nd St.	Trees:		
	<i>Populus nigra 'Italica'</i> ,	Lombardy Poplar	30' O.C.
	<i>Arecastrum romanzoffianum</i> ,	Queen Palm	30' O.C.
	Groundcover:		
	<i>Lantana montevidensis</i> ,	Trailing Lantana	
	<i>Rosmarinus o. 'Prostratus'</i> ,	Dwarf Rosemary	
Sunrise St.	Trees:		
	<i>Jacaranda mimosifolia</i> ,	Jacaranda	
	<i>Calodendrum capense</i> ,	Cape Chestnut	
	<i>Albizia julibrissin</i> ,	Silk Tree	
	<i>Arecastrum romanzoffianum</i> ,	Queen Palm	30' O.C.
	<i>Archontophoenix cunninghamiana</i> ,	King Palm	
	Shrubs:		
<i>Dietes bicolor</i> ,	Fortnight Lily		
	Groundcover:		
	<i>Lantana montevidensis</i> ,	Trailing Lantana	
	<i>Rosmarinus o. 'Prostratus'</i> ,	Dwarf Rosemary	
I-94 Boundary	Trees:		Naturalistic Planting
	<i>Eucalyptus cladocalyx</i> ,	Sugar Gum	
	<i>Phoenix canariensis</i> ,	Canary Island Date Palm	
	Shrubs:		
	<i>Arctostaphylos densiflora</i> ,	Manzanita	
<i>Acacia redolens</i> ,	Prostrate Acacia		
<i>Sambucus mexicana</i> ,	Mexican Elderberry		
	Groundcovers:		
	<i>Baccharis pilularis</i> ,	Prostrate Coyote Brush	
	<i>Gazania splendens</i> ,	Mixed Color Gazania	
I-15 Boundary	Trees:		Naturalistic Planting
	<i>Platanus racemosa</i> ,	California Sycamore	
	<i>Quercus agrifolia</i> ,	Coast Live Oak	
	<i>Washingtonia robusta</i> ,	Mexican Fan Palm	
	Shrubs:		
	<i>Baccharis viminea</i> ,	Mulefat	
<i>Lonicera japonica</i> ,	Japanese Honeysuckle		
<i>Sambucus mexicana</i> ,	Mexican Elderberry		

# I. Plant Materials for Individual Lot Development

	Botanical Name	Common Name
Trees	<i>Cupaniopsis anacardioides</i>	Carrotwood
	<i>Eucalyptus citriodora</i>	Lemon Scented Gum
	<i>Eucalyptus lehmanii</i>	Bushy Yate
	<i>Ficus species</i>	Ornamental Fig
	<i>Luquidambar styraciflua</i>	Sweetgum
	<i>Magnolia grandiflora</i>	Southern Magnolia
	<i>Melaleuca quinquenervia</i>	Cajeput Tree
	<i>Pinus canariensis</i>	Canary Island Pine
	<i>Pinus halepensis</i>	Aleppo Pine
	<i>Podocarpus gracilior</i>	Fern Pine
	<i>Pyrus kawakamii</i>	Evergreen Pear
	<i>Schinus terebinthifolius</i>	Brazilian Pepper
	<i>Tristania conferta</i>	Brisbane Box
	<i>Ulmus parvifolia</i>	Evergreen Elm
Shrubs	<i>Arbutus unedo</i>	Strawberry Tree
	<i>Dicksonia antarctica</i>	New Zealand Tree Fern
	<i>Eleagnus pungens</i>	Silverberry
	<i>Hebe species</i>	Hebe
	<i>Nandina domestica</i>	Heavenly Bamboo
	<i>Nerium oleander</i>	Oleander
	<i>Photinia fraseri</i>	Frazer Photinia
	<i>Pittosporum tobira "Variegata"</i>	Variegated Tobira
	<i>Pittosporum tobira</i>	Tobira
	<i>Podocarpus macrophyllus</i>	Yew Pine
	<i>Raphiolepis indica</i>	India Hawthorn
	<i>Trachelospermum jasminoides</i>	Star Jasmine
	<i>Xylosma congestum</i>	Shiny Xylosma
Vines	<i>Bougainvillea braziliensis</i>	Bougainvillea
	<i>Clytostoma callistegioides</i>	Lavender Trumpet Vine
	<i>Jasminum polyanthum</i>	Jasmine
	<i>Parthenocissus tricuspidata</i>	Boston Ivy
	<i>Doxcanthus unguis-cati</i>	Cat's Claw Vine
Ground Cover	<i>Ceanothus griseus horizontalis</i>	Carmel Creeper
	<i>Fragaria chiloensis</i>	Ornamental Strawberry
	<i>Gazania "Sunrise Yellow"</i>	Gazania
	<i>Hedera helix</i>	English Ivy
	<i>Vinca minor</i>	Periwinkle

## VII. PERMITTED USES

The Gateway Center West Design Guidelines represent the regulations of the Southeast San Diego Planned District Ordinance and the Declaration of Covenants, Conditions and Restrictions of the Dells. The following discussion is a synopsis of regulations and permitted uses. For detailed information regarding development please refer to both of the aforementioned documents. The Gateway Center West Design Guidelines have been assembled to provide a basic understanding of the regulations and permitted uses.

The industrial zones in the Southeast San Diego Planned District are in two specific groups. The assignments correspond to the land use designations of the Southeast San Diego Community Plan. Industrial properties are zoned Light Industrial, I-1, or the Industrial Park, I-2, which is tailored for the establishment of larger industrial complexes. The I-1 and I-2 zones are intended to provide quality development, decrease land use conflicts and provide maximum employment opportunities.

The majority of the Gateway Center West site is designated I-1 (see zoning map on page 53). The SDG&E facility is zoned I-2. The following is a discussion of the Gateway Center West's zones:

### Light Industrial I-1 Zone

The Light Industrial I-1 Zone is intended to provide for a wide range of manufacturing, light industrial uses and certain "heavy" commercial uses such as lumber yards which are not commonly found in shopping centers.

The standards and regulations in this zone are designed to permit development and uses of property in a manner that is consistent with efficient industrial operation, while at the same time providing proper safeguards for adjoining industrial and non-industrial properties.

### Industrial Park I-2 Zone

The Industrial Park I-2 Zone is a zone with development criteria that is tailored for the establishment of larger industrial complexes. This zone is also intended to accommodate a wide range of manufacturing uses in addition to some related office uses that can experience a harmonious development in larger office/industrial park complexes.

Properties located along the west side of 32nd Street Between F Street and Market Street have been included for consideration in Gateway Center West Design Guidelines so the Redevelopment Agency can evaluate the potential of rezoning of these areas to industrial.

The existing zones for this area is CSF-2, CSF-3 and MF-3000.

### CSF Zone

The CSF zone is intended to allow for commercial strip development with parking to the front or side of the building. This zone is suggested to accommodate existing development patterns.

### MF-3000

The multiple-family zones are intended to provide multi-family housing at varying densities.

A. Permitted Uses by Zone

No building or improvement, or portion thereof, shall be erected, constructed, converted, established, altered or enlarged, nor shall any premise be used except for one or more of the uses listed for applicable zones in the following Permitted Use Matrix:

Legend: P = Permitted  
 SP = Special Permit  
 - = Not Permitted

<u>Permitted Industrial Uses</u>	<u>Industrial Zones</u>	
	<u>I-1</u>	<u>I-2</u>
<u>Residential</u>		
Mobilehome (1) Temporary Watchman	SP	SP
<u>Commercial</u>		
Advertising; Secretarial; Telephone Service	P	P
Agencies for tickets; travel; car rental	P	P
Ambulance Service	P	-
Animal Hospital	SP	-
Antique Shops	P	-
Automobile/Truck sales & rental (usable vehicles)	P	-
Automobile Wash Establishments	P	-
Automobile Paint and Body Shops	SP	-
Automobile Gas and Service Stations	SP	SP
Bakeries and Bakery Products	P	-
Banks; Savings & Loans	P	P
Boat Sales Agencies	P	-
Building Materials Stores	P	-
Business Machine Sales, Service & Display	P	-
Cleaning & Dyeing Works (carpets & upholstery)	P	-
Communication Facilities	P	-
Confectioneries	P	-
Curtain; Drapery; and Upholstery Shops	P	-
Custom upholstery, flooring & apparel shops	P	-
Delicatessens	P	-
Drafting and Blue Print Services	P	-
Dry Cleaning and Laundry Agencies	P	-
Electronic Data Processing	P	-
Employment Agencies	P	-
Equipment and Tool Rental (No man-ridden equipment)	P	-
Feed Stores	P	-
Financial Institutions	P	-
Florist Supplies	P	-
Frozen Food Lockers	P	-
Furniture Stores	P	-
Gymnasium and Health Studios	P	-
Hardware Stores	P	-
Hospital/Veterinary Clinic	SP	-

Permitted Industrial Uses

Industrial Zones

	<u>I-1</u>	<u>I-2</u>
<u>Commercial Continued</u>		
Ice Delivery Stations	P	-
Interior Decorators	P	-
Labor Unions & Trade Associations (no hiring halls)	P	-
Lithography Shops	P	-
Locksmith Shops	P	-
Medical; Dental; Biological; X-Ray Laboratories	P	-
Medical Appliance Sales	P	-
Motor Vehicle (sale of new retail parts & accessories)	P	-
Moving and Household Storage Facilities	P	-
Newspaper Plants	SP	-
Nurseries (Plants)	P	-
Office Furniture and Equipment Sales	P	-
Offices; Business and Professional	P	P
Parking Lots-Commercial	SP	-
Plumbing Shops	P	-
Post Offices	SP	SP
Public Body Operated Buildings and Uses	SP	SP
Radio; TV and Home Appliance Repair	P	-
Radio and TV Broadcasting Studios	SP	-
Recreational Facilities (2,500 sq. ft. max. FAR)	P	-
Restaurants (excluding sale of intoxication beverages, drive-through and incidental entertainment)	P	-
Restaurants (including sale of intoxicating beverages)	SP	-
Studios for teaching dance, music and art	SP	-
Trade and Business Schools	P	-
Trailer Sales Agencies	P	-
Wholesaling or warehousing of goods and merchandise, associated with the primary on-site use provided that the floor area occupied for such use per establishment does not exceed 25% of the building gross floor area.	P	-
<u>Industrial</u>		
Aircraft manufacturing and assembly	P	P
Aircraft parts other than engines	P	P
Apparel belts	P	P
Apparel except leather and fur goods	P	P
Audio products	P	P
Awnings - metal, wood or canvas	P	P
Bags, except textile bags	P	P
Bakery products	P	P

Permitted Industrial UsesIndustrial ZonesIndustrial Continued

	<u>I-1</u>	<u>I-2</u>
Beverages	P	P
Brooms and brushes	P	P
Cabinet making	P	P
Coated, plated and engraved metal	SP	SP
Communication equipment	P	P
Concrete and clay products	SP	SP
Confectionery and related products	P	P
Costume jewelry, costume novelties button, and miscellaneous notions	P	P
Cut stone and stone products	P	P
Cutlery, hand tools and general hardware	P	P
Diecut paper and paperboard, and cardboard	P	P
Dress and work gloves, except knit and leather products	P	P
Drugs	P	P
Electronic lighting and wiring equipment	P	P
Electric industrial apparatus	P	P
Electronic computing equipment other than desk top machines	P	P
Envelopes	P	P
Fabricated textile products	P	P
Fabricated wire products	P	P
Farm machinery and equipment	P	P
Furniture and fixtures	P	P
Glass containers	P	P
Glassware, pressed and blown	P	P
Guided missiles and space vehicles	P	P
Hats, caps and millinery	P	P
Jewelry, silverware and plated ware	P	P
Laboratories - Research, Development, Testing	SP	SP
Lamp shades, except metal and glass	P	P
Luggage	P	P
Metal cans	P	P
Mill work	P	P
Miscellaneous apparel and accessories	P	P
Motor vehicles, parts and accessories	P	P
Mortician's goods	P	P
Musical instruments and parts	P	P
Office machines	P	P
Paperboard containers and boxes	P	P
Pens, pencils and other office and artists' materials	P	P
Perfumes, cosmetics and other toilet preparations	P	P
Plumbing fixtures and heating apparatus	P	P
Pottery and related products	P	P
Professional, scientific and controlling instruments	P	P

Permitted Industrial Uses

Industrial Zones

Industrial Continued

	<u>I-1</u>	<u>I-2</u>
Photographic and optical goods, watches and clocks	P	P
Radio and television receiving sets, except communication types	P	P
Roasted coffee and coffee products	P	P
Robes and dressing gowns	P	P
Service industry machines	P	P
Sighting and fire-control equipment	P	P
Special industry machinery, except metal-working machinery	P	P
Toys, amusements, sporting and athletic goods	P	P
Umbrellas, parasols and canes	P	P
Wooden containers	P	P

Establishments engaged in the wholesale distribution of the following:

Automotive equipment	P	-
Drugs, chemicals and allied products	P	-
Dry goods and apparel	P	-
Flowers and florist supplies	P	-
Groceries and related products, except poultry, and poultry products, fish and seafood, and fruit and vegetables	P	-
Electrical goods	P	-
Hardware, plumbing, heating equipment and supplies	P	-
Machinery, equipment and supplies, except farm machinery and equipment	P	-
Tobacco and tobacco products	P	-
Beer, wine and distilled alcoholic beverage	P	-
Paper, paper products and kindred supplies	P	-
Office and home furniture and furnishings	P	-

Establishments engaged in the following:

Data processing services	P	P
Motion picture production	P	-

The following establishments:

Carpenter shops	P	-
Metal working shops	SP	-
Lumber yards	P	-
Machine shops	P	-
Public utility substations	P	-

Permitted Industrial Uses

Industrial Zones

Industrial Continued

I-1      I-2

Regional and headquarters offices of  
businesses, industries and governmental  
agencies  
Storage garages  
Welding shops

P      P  
P      -  
SP      -

The following business and professional establishments:

Accountants  
Architects  
Attorneys  
Contractors  
Engineers  
Financial institutions  
Insurance agencies

P      P  
P      P  
P      P  
P      -  
P      -  
P      P  
P      -

Permitted Industrial Uses

Industrial Zones

I-1      I-2

The following business and professional establishments Continued:

Photographers  
Real Estate brokers  
Surveyors  
Graphic artists  
Business machine sales, display and service  
Drafting and blueprinting  
Electronic data processing  
Tabulating and record-keeping services  
Labor unions and trade associations  
Addressing and secretarial services

P      -  
-      -  
P      -  
P      -  
P      -  
P      P  
P      P  
P      P  
P      -  
P      P

Any other use which the Planning Commission may find to be similar in character or compatible to the uses permitted in the specific zone or zones. The adopted resolution embodying such findings shall be filed in the Office of the City Clerk. Any other use allowed with a Conditional Use Permit with the City Council as decision maker per Section 101.0510-C.4.

**B. Industrial Zone Development Standards**

1. Setback Requirements

Zone	Front Yard	Side Yard (Interior)	Side Yard (Street)	Rear
I-1	10"	0'	10'	15'
I-2	25'	10% of width 30' max	25'	25'

Interior side yard abutting residential zoned property - 25'.

Rear yard abutting residentially zoned property - 25'.

Interior side yard abutting residentially zoned property - 50'.

Rear yard abutting residential zoned property - 50'.

2. Maximum Building Height, Lot Coverage and Floor Area Ratios

Zone	Coverage	Building Height*	F.A.R.
I-1	50%	--	1.5
I-2	40%	--	2.0

\* Any portion of a building on-site within 200 feet of residentially zoned property shall not exceed 30 feet in height.

3. Signage

The sign provisions shall comply with the criteria contained within the City-Wide Sign Regulations - Chapter X, Article 1, Division 11, of the Municipal Code.

4. Off-Street Parking Regulations

Every site used for one or more of the permitted uses listed in the permitted industrial use matrix shall be provided with a minimum of off-street parking spaces on the same lot or premises as follows:

- For business and professional office uses - one parking space for each three hundred (300) square feet of gross floor area.
- For incidental businesses and offices - one parking space for each four hundred (400) square feet of gross floor area.
- For light industrial/manufacturing uses - one (1) parking space for each four hundred fifty (450) square feet of gross floor area.

The agency may grant variance to parking requirements for some existing developed properties that do not have adequate space for on-site parking. Variances will require agency to make the following findings:

- There is no acceptable on-site location to meet the parking requirements.
- There is sufficient on-street parking for the business in question.
- Granting of the variance will not provide a special privilege or right that no one else in the area enjoys.
- Granting of the variance will not adversely affect the community plan and PDO.

5. Site Landscape Requirements

Prior to the use and occupancy of any premises, the property shall be landscaped in accordance with the provision of the City-Wide Landscape Regulations in Chapter X, Article 1, Division 7 of this Code.

Landscaping requirements contained herein are required for all industrial uses which existed on the effective date of this Division and shall be installed within five years of the effective date of this Division.

6. Off-Street Loading Facilities

Loading or unloading facilities shall be so sized and located so as to not permit trucks in required front or street side yards during loading and unloading activities.

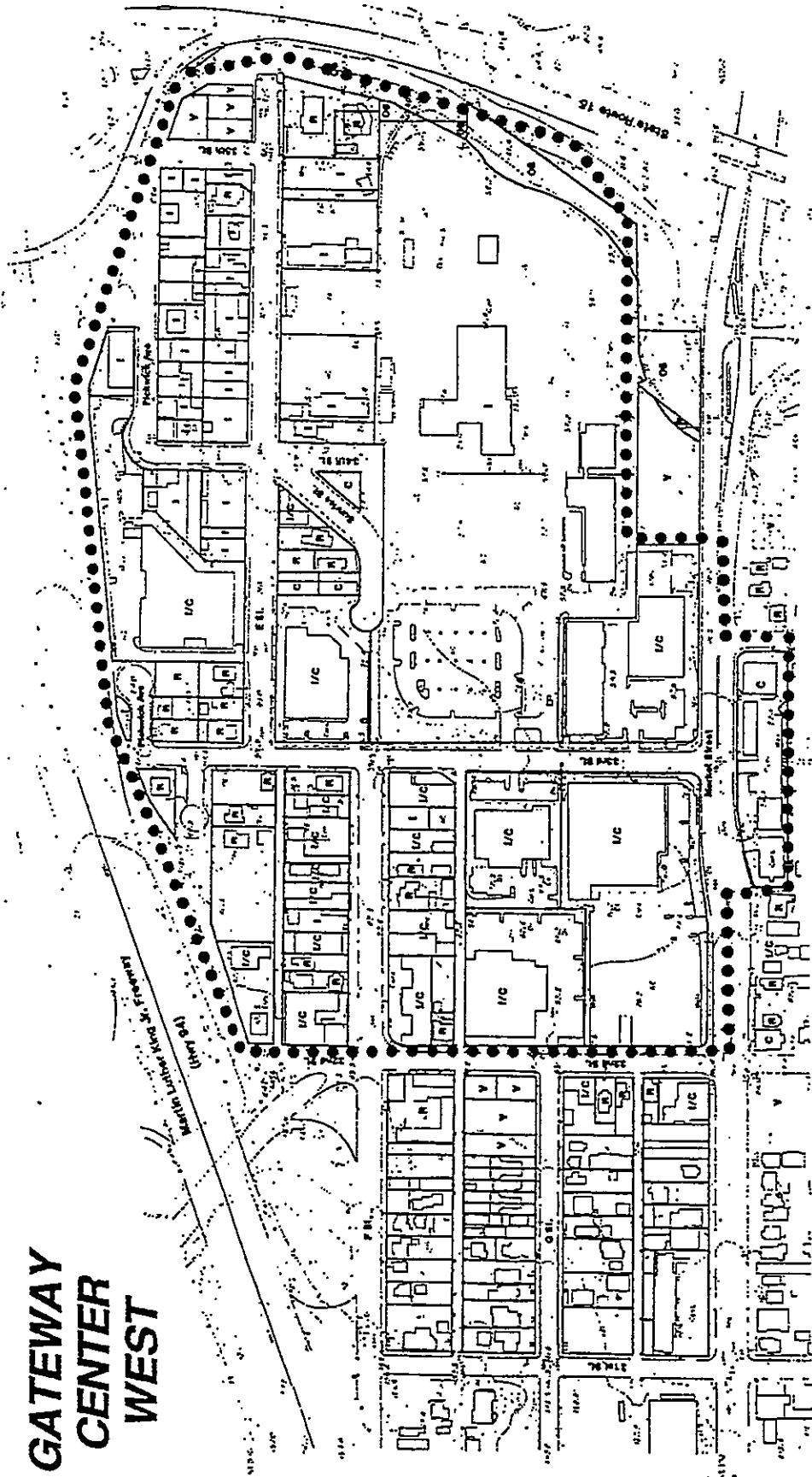
7. Outdoor Eating Areas

Within the site plan and building design, incorporate outdoor eating and activity areas. These areas shall be accessible to occupants from inside and outside the building. They shall be incorporated into the landscape. Employees shall be encouraged to stay on site during breaks and lunches. Outdoor eating areas typically provide seating, trash receptacles, lighting and sometimes wide screening to make the space usable. Outdoor eating areas should be proportional to the size of the work force. No less than 120 square feet shall be dedicated for outdoor eating spaces. The outdoor eating areas can be included within landscape setbacks and minimum coverages.

8. Outdoor Recreational Facilities

Recreational opportunities are encouraged to be incorporated into individual projects. Additional uses for parking lots are permitted such as basketball courts or other activities which require a hard surface court.

# GATEWAY CENTER WEST



**LEGEND**

	INDUSTRIAL/COMMERCIAL
	RESIDENTIAL
	OPEN SPACE/VACANT LAND

**NOTE:**  
 ALL LAND USES SHOWN REPRESENT APPROXIMATE  
 45% OF THE PLAN IS FOR CONCEPTUAL  
 ANALYSIS ONLY



## LAND USE MAP

Project Boundary

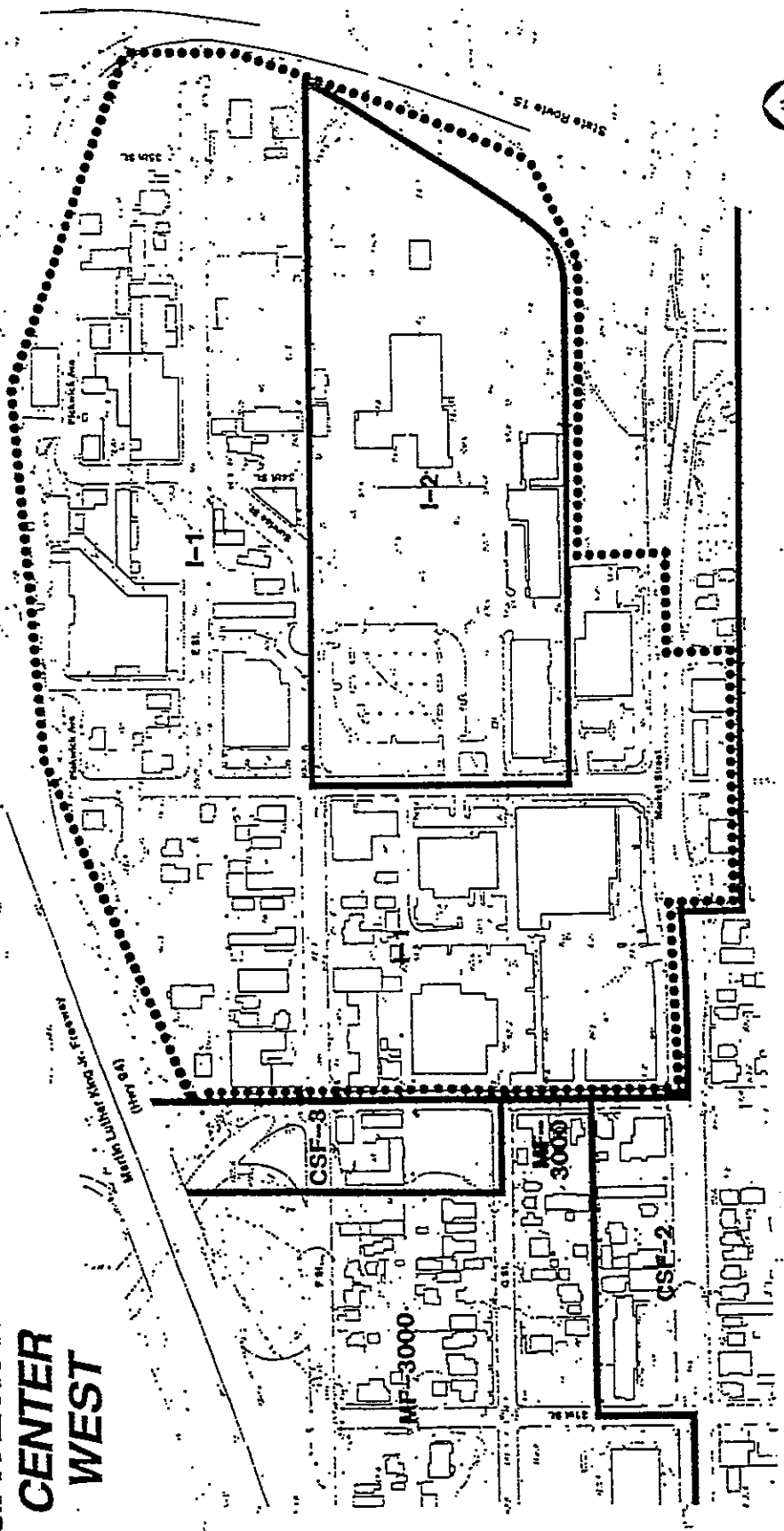


**HCH PARTNERS**  
 ARCHITECTURE  
 PLANNING  
 INTERIOR

477 Broadway Avenue  
 San Diego, CA 92101-1640  
 (619) 218-5100  
 FAX (619) 218-9724

January 1987

# GATEWAY CENTER WEST



**HCH PARTNERS**  
ARCHITECTURE  
ENGINEERING  
INTERIORS  
PLANNING

4977 Viewridge Avenue  
San Diego, CA 92122 (619) 591-7810  
FAX (619) 591-0728  
© 1997

Project Boundary

ZONING MAP

## VIII. SUBMITTAL AND REVIEW PROCESS

The following represents the development review process for projects within Gateway Center West. These procedures have been established in order to facilitate development, but also to ensure the highest quality projects.

Two separate procedures have been established for the review of development proposals within Gateway Center West. The first shall apply to projects which involve Redevelopment Agency owned land or an owner participation agreement between the property owner and the Redevelopment Agency. The second shall apply to projects developed without Agency assistance. Both of the following procedures shall apply to all new developments and additions of greater than 25%. Rehabilitation and additions of less than 25% of the existing floor area, shall be subject to administrative review by SEDC and the City of San Diego Planning Department.

### A. Agency Projects

#### 1. Submittal Requirements

Schematic plans and related documents for the development of property within Gateway Center West shall be submitted by the Applicant to SEDC for Redevelopment Agency approval in accordance with Section XXX of the Gateway Center West Redevelopment Plan. All submittals shall include the following:

- a. Site plans shall indicate:
  - Name of the project and location
  - Name and address of the owner or person with financial interest in the project
  - Location, name and width of adjacent and on-site driveways, streets, alleys, highways, easements and walkways
  - Building location, including setbacks
  - Land area of the site
  - Building floor area, height, coverage and uses
  - Parking - number and location
  - Location of walls and fences including heights and materials
  - Existing topographic features (provide on a separate plan if necessary)
- b. Landscape Plans which indicate the location of landscaping/hardscaping, the types of plants, a detail indicating the type of materials and the design of any hardscape and the proposed sizes of all landscaping.
- c. Building elevations which illustrate a view from all sides and indicate the type of construction, the proposed exterior building materials, height and proposed colors.
- d. Floor plans indicating a breakdown of the types of uses.
- e. Sections which provide elevations of all fencing and walls.

2. Discretionary Review

At least 30 days prior to Planning Commission review, SEDC shall provide the Planning Department with a copy of all information regarding the proposed development. The Southeast Development Committee shall also be provided with a set of plans for their review.

The Planning Commission shall review the schematic plans and make a recommendation to the Redevelopment Agency within 30 days of the receipt, by SEDC, of a complete application and completing of all required environmental review. No date for Agency action shall be scheduled until a hearing before the Planning Commission has been set. The SEDC, the Planning Department, the Planning Commission, and the Agency have the authority to place conditions on the approval of any project.

3. Preliminary and Final Plans

Following Agency approval of the schematic plans, preliminary and final plans and related documents shall be submitted to SEDC for approval. The SEDC shall either approve, approve with conditions or disapprove such plans and documents. In the event that SEDC approves with conditions or disapproves such plans, the applicant may appeal such a decision by filing a written notice of intent to appeal with the Secretary of SEDC. Said notice of appeal shall specify the grounds for appeal and be submitted within ten (10) days of receipt of a written decision from SEDC. The final decision shall be made a noticed public meeting of the SEDC Board of Directors.

B. Other Projects

All projects which are being developed without the assistance of the Redevelopment Agency, shall be subject to the processing requirements as established in the Southeast San Diego Planned District Ordinance (San Diego Municipal Code, Chapter X, Section 103.1703). The Southeast Economic Development Corporation shall have an advisory role, but the City of San Diego Planning Department will be responsible for the processing of the discretionary permit.

## IX. INFRASTRUCTURE

Several engineering studies have been performed for the project area. Graves Engineering prepared a study in 1982. Poutney and Associates prepared a capital improvements survey in 1985. During the mid 1980's HCH Partners completed several preliminary land development cost estimates for SEDC and individual projects within Gateway Center West. Also, HCH Partners recently made a preliminary visual reconnaissance of existing street surface conditions and a review of 100 scale City utility drawings. Therefore, our recommendations are based upon previous engineering reviews, past experience, and recent observations within Gateway Center West.

### A. Existing Conditions:

#### 1. Streets

All existing streets within the project are paved. These streets, however, are classified either improved or partially improved. Improved streets are either paved with concrete or asphalt and have curbs and gutters. Sidewalks may or may not be present along improved streets. Partially improved streets have been surfaced with either asphalt or concrete but contain no curbs, gutters along part or all the street length.

The following table indicates the classification of existing streets:

<u>Improved</u>	<u>Partially Improved</u>
Market Street	Pickwick Avenue
32nd Street	E Street
33rd Street	35th Street
34th Street	
F Street	
Sunrise Street	

35th Street is unimproved and so is 34th Street south of E Street. The alley parallel to and north of F Street between 32nd and 33rd Street is partially improved.

#### 2. On-Street Parking

On street parking is permitted throughout the project area except along portions of Market Street. Parking on F and E Street is diagonal along the south side of both streets. Preliminary discussions with the City of San Diego's Transportation Planning Division indicated that this practice was acceptable.

#### 3. Sidewalks

Sidewalks exist throughout most of the project area. Pedestrian access is excellent along Market, 33rd, F and 32nd Streets. There is a lack of continuous sidewalks along E Street, however, which reduces pedestrian accessibility.

#### 4. Bus Transit

Bus stops are currently located along Market Street near the intersection of 33rd Street.

## 5. Utilities

The majority of the existing water, sewer and storm drain utilities within the project area were identified in previous studies as substandard. Recent redevelopment efforts since 1985 along 32nd, Market and 33rd Street, however, have generally upgraded adjacent utilities to current City standards. The existing water, sewer, and storm drain utilities along E Street and Pickwick Avenue are substandard. This portion of the project area has utilities built more than forty years ago. These utilities were designed for residential uses not industrial uses. Of particular concern are the existing inadequate storm drain facilities.

Numerous overhead electrical and telephone lines criss-cross the project area along E Street. The sizes of the electrical lines vary from local transmission 12kv to high voltage transmission facilities of 69kv or larger.

## 6. Street Lighting

Existing lighting system within the project area is substandard and does not conform to the City's uniform street lighting standards for number and spacing. The City requires street lights to be spaced 120 feet and staggered alternatively from one side to the other side of the street.

## B. Proposed Project

### 1. Streets

In order to modify and improve the current street system it is recommended that Market and 33rd Streets, and F and 32nd Streets remain the primary and secondary entries into Gateway Center West, respectively. The preferred concept plan requires that 33rd Street and a portion of Pickwick Street north of E street be vacated. The 33rd Street intersection with E Street will be reconstructed as a knuckle. Sunrise Street south of E Street will be vacated and replaced by a private driveway. Pickwick avenue east of 34th Street, north of E Street, will become a cul de sac with an approximate length of 350 feet. E street will also become a cul de sac ending approximately 600 feet east of Sunrise Street. When completed this proposed circulation system will simplify the current street pattern and make it more efficient (See proposed infrastructure improvements map exhibit on page 60).

### 2. On-Street Parking

On-street parking is proposed to be retained similar to existing conditions. Improved diagonal parking is proposed for the south side of E and F Streets (See East-West corridors under Landscape Standards). Also, diagonal parking is proposed for the south side of Pickwick Avenue, east of 34th Street. Parallel parking is proposed for the north side of E and F Streets (and other locations particularly mentioned), Pickwick Avenue, however, will not have parallel or any type of on-street parking on it's north side. Market Street will not be affected by this plan.

### 3. Sidewalks

Pedestrian circulation is an integral component of the circulation system. The major pedestrian corridors are along F Street, 33rd Street and E Street. 33rd Street is the major pedestrian route linking Market Street to the balance of the project. E and F Streets provide the major East-West access through the project area. Improvement to the sidewalks along E street are a major emphasis of this project.

### 4. Bus Transit

Access to bus transit should be expanded within the project if possible. A bus loop is proposed from Market along 33rd Street, F Street to 32nd Street and returning to Market Street. Bus stops should be installed at the intersections of 33rd and F Street and 32nd and F Street. Coordination with MTDB will be required by the Agency to facilitate this modified bus/bus stop route.

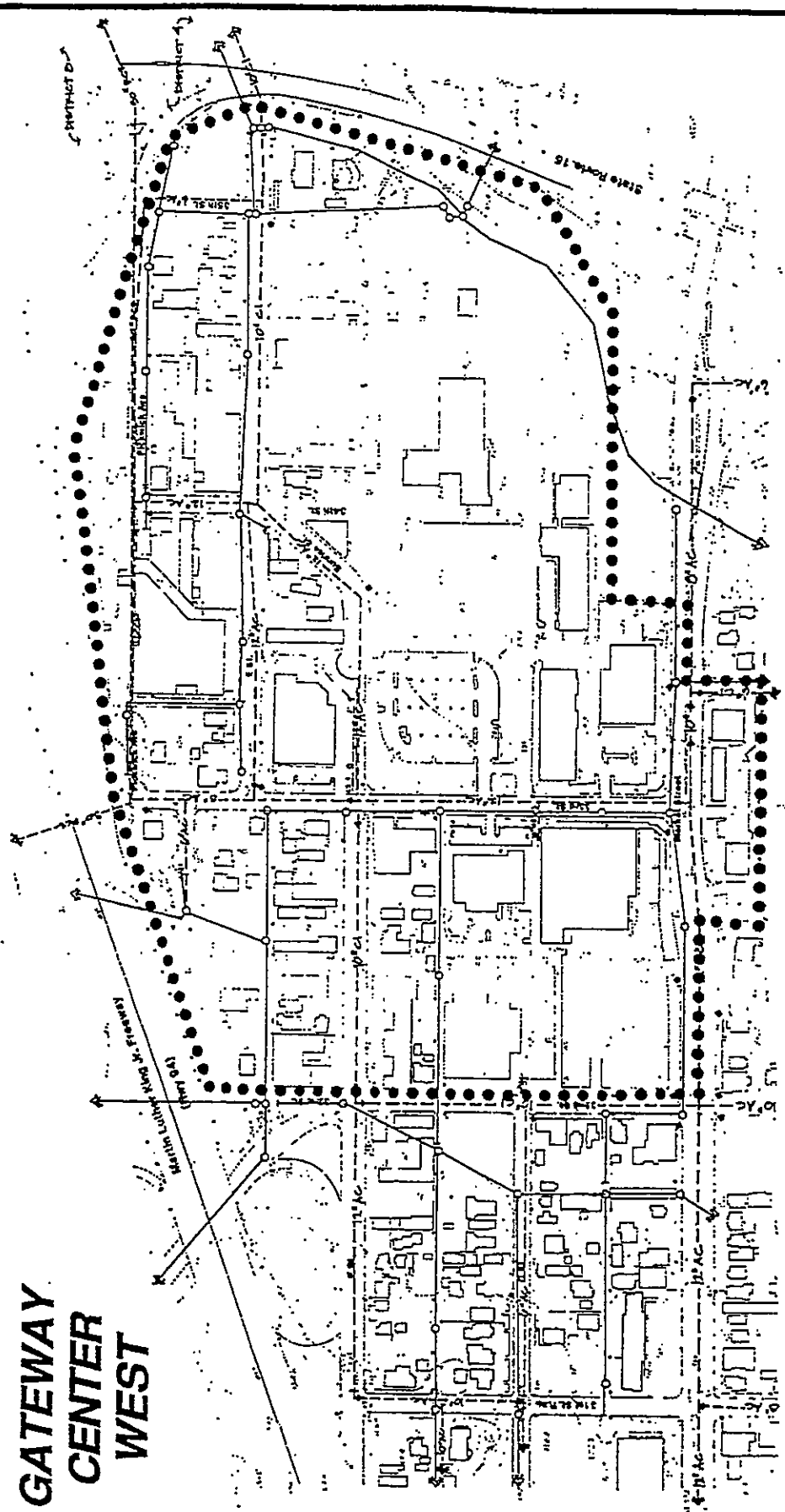
### 5. Utilities

Utilities, including water, sewer and storm drains will be upgraded concurrent with proposed street improvements to City standards. Some overhead telephone and electrical distribution lines can be undergrounded during street reconstruction. Electric transmission lines higher than 69kv are typically not undergrounded. 33rd Street provides an example overhead electrical transmission lines that are typically not undergrounded due to the high construction costs involved.

### 6. Street Lighting

Street lighting within the project area will be upgraded to current City standards as new development occurs. Areas that do not anticipate new development will require the Agency to develop a lighting district to fund and maintain the street lights. The style and character of the street lights can be found in the Architectural Guidelines, Section V, under Light Standards, page 19.

# GATEWAY CENTER WEST



## LEGEND

- WATER LINE
- SEWER LINE
- LIGHT POLES

NOTE:  
ALL UTILITIES SHOWN REPRESENT APPROXIMATE LOCATIONS. THIS PLAN IS FOR CONCEPTUAL ANALYSIS ONLY.



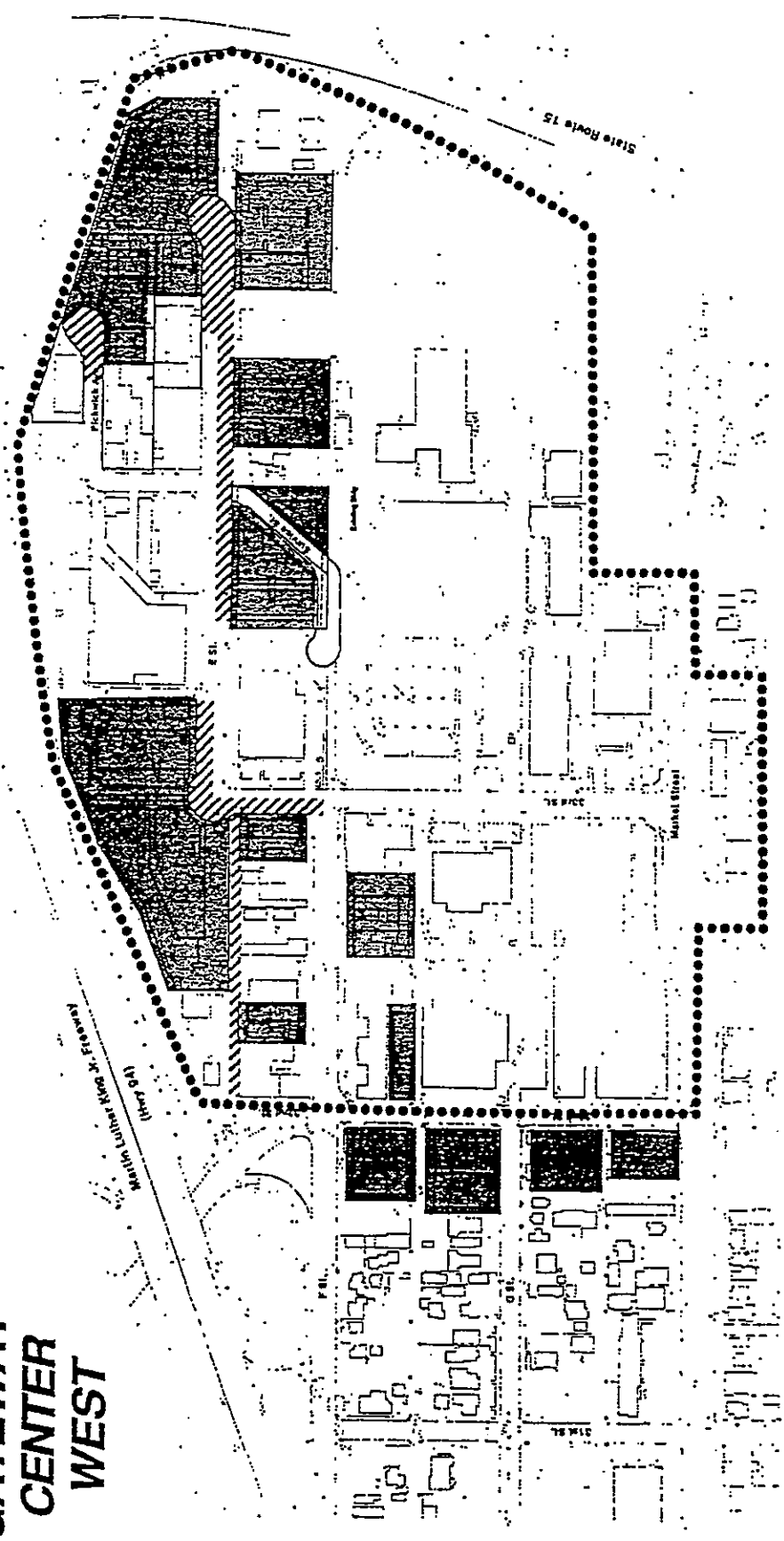
HOCH PARTNERS  
ARCHITECTURAL  
ENGINEERS  
SURVEYORS  
417 Newpage Avenue  
San Diego, CA 92131-1447  
619 594 5150  
FAX 619 594 0258

0410

January 1987

## UTILITIES MAP

# GATEWAY CENTER WEST



- LEGEND**
-  Lots to be Redeveloped
  -  Street Improvements
  -  Project Boundary

4477 Newbury Avenue  
 San Diego, CA 92132-3671  
 (619) 594-3300  
 FAX (619) 594-8254

10001100000  
 AUGUST 1997  
 HCH  
 HAVENLITS  
 MAR 15 1997

INFRASTRUCTURE AND LOT IMPROVEMENTS